

City of Waterloo, Illinois Downtown ADA Transition Plan

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Introduction

The purpose of this plan is to evaluate the pedestrian travel zones located within the City of Waterloo downtown area, ensuring that the public right-of-way is accessible in all situations. Under this premise, the City of Waterloo strives to provide equal transportation paths for those with disabilities, as outlined in the Americans with Disabilities Act (ADA). This Transition Plan pinpoints potential hazards and prioritizes these physical barriers in such a way as to schedule improvements to the paths of travel in the most logical and effective manner. This formal Transition Plan serves as the City of Waterloo's commitment to improving overall pedestrian accessibility in the downtown area.

Legal Requirements

The federal registration known as the Americans with Disabilities Act (ADA), enacted on July 26, 1990, provides comprehensive civil rights protections to persons with disabilities in the areas of employment, state and local government services, and access to public accommodations, transportation, and telecommunications. Title II of the ADA specifically applies to state and local governments, referred to as "public entities", and their programs and services. Title II Article 8, requires public entities to take several steps designed to achieve compliance. The Transition Plan used to implement compliance must include:

1. A list of the physical barriers in a public entity's facilities that limit the accessibility of its programs, activities, or services to individuals with disabilities.
2. A detailed outline of the methods to prioritize the barriers removal and make the facilities accessible.
3. The schedule for taking the necessary steps to achieve compliance with Title II.
4. The name of the official responsible for the plan's implementation.

Pedestrian Access Route Evaluation

Before developing an approved ADA Transition Plan, the City of Waterloo administered an examination of its curb ramps in the downtown area near the courthouse. The Survey Department from HMG Engineers, Inc. conducted an inspection utilizing state-of-the-art geographic information system (GIS) technology to capture and analyze specific data. A detailed inventory from this collected data was then compiled for determination of compliant and non-compliant curb ramps. The following criteria was considered:

Curb Ramps

1. Is there a curb ramp?
2. Are there detectable warnings of opposing color installed?

3. What is the length of the ramp?
4. What is the running slope of the ramp?
5. What is the width of the ramp?
6. What is the cross slope of the ramp?
7. If the running slope is greater or equal to 5.0%, is there a landing area that is a minimum of 48 inches by 48 inches and less than 2.0% in all directions?
8. What is the curb ramp's condition?

Once identified, all features were imported into the City of Waterloo's geodatabase, and a comprehensive map of said features was composed in order to more easily identify areas of high priority. To provide further evidence of these barriers, supplemental street-level photographs were taken in sequence with every point.

Scoring Criteria

From the data obtained in the curb ramp evaluation, each feature was given a score based on its compliance with a predetermined set of standards. At each intersection, each curb ramp was assigned an individual score, which was then averaged with the adjacent curb ramp at that said quadrant of the intersection. This average gives each quadrant of the intersections an overall score. Locations falling under the "no ramp" category received individual scores of 55 points and compliant ramps were treated as zero points. The grading scale for individual curb ramps adhered to the following standards, with the maximum set at 55 points:

RUNNING SLOPE: 0 pts. = 0.0% - 8.3%
 5 pts. = 8.4% - 10.4%
 10 pts. = 10.5% - 12.5%
 15 pts. = 12.6% +

CROSS SLOPE: 0 pts. = 0.0% - 2.0%
 5 pts. = 2.1% - 4.0%
 10 pts. = 4.1% - 6.0%
 15 pts. = 6.1% +

DETECABLE WARNING: 0 pts. = YES
 10 pts. = NO

SIDEWALK WIDTH: 0 pts. = \geq 4 ft.
 5 pts. = $<$ 4 ft.

**LANDING AREA:
 (IF NECESSARY)** 0 pts. = YES / EXEMPT
 5 pts. = NO

CONDITION: 0 pts. = GOOD
 3 pts. = FAIR
 5 pts. = BAD

After calculating all point totals, each quadrant of the intersection was given a priority value from one (1) to six (6). A grade of one (1) resembles the most immediate attention and six (6) resembles the least immediate attention. The priority values assigned to each quadrant of the intersection was based on the following scale:

GREEN { •6 = Score 0-16
 •5 = Score 9-16.5

YELLOW { •4 = Score 16.6-24
 •3 = Score 25-32.5

RED { •2 = Score 32.6-40
 •1 = Score Greater than 40

Policies

In order to uphold the best interests of its citizens, the City of Waterloo will honor all opinions through a public grievance process. Upon receiving a request, additional consideration will be provided to the situation, possibly resulting in a reprioritization of the improvement schedule if deemed necessary. If desiring to file a grievance pertaining to the accessibility of a sidewalk or curb ramp, a citizen should contact the city's ADA Coordinator, presenting in writing a copy of the Complaint/Grievance Form with a thorough description and precise location of the concern. The ADA Coordinator is then responsible for relaying the formal request to the appropriate City of Waterloo employee, who will determine the proper action to be taken. Any accumulated public grievances will be kept on file for future access.

Training

The ADA coordinator is responsible for educating the appropriate City of Waterloo employees to ensure the newest ADA guidelines are understood and utilized during construction operations.

Estimated Construction Costs

Required Improvement	Improvement Cost
Install new ADA compliant ramp without curbing	\$750
Install new ADA compliant ramp with curbing	\$1,050
Replacing existing ramp with ADA compliant ramp	\$850
Replacing existing ramp & curb with ADA complaint ramp & curb	\$1,300
Replacing existing section of sidewalk/trip hazards	\$35/linear foot

The table above represents the estimated construction costs for each non-compliant scenario. Given each location is unique, assumptions were made in order to establish unit costs of each scenario for the City's budget. Assumptions are new 4 foot wide by 10 foot long sidewalk for ramps, detectable warnings, new 10 foot long curbing where necessary, and removals of existing features where necessary.

Schedule

By reserving funds exclusively with the intention of supporting the Downtown ADA Transition Plan, the City of Waterloo will endeavor to improve the quality and accessibility of its public right-of-way within reason. According to *Accessibility Guidelines for Pedestrian Facilities in the Public Right-of-Way*, "compliance is required to the extent practicable within the scope of the project"; there will be times when it is technically infeasible to provide compliance but the compiled inventory ratings fail to recognize this. The City of Waterloo will focus on zones containing features that are both of high priority and plausibly repairable before proceeding to lower priorities unless it receives a specific grievance from the public.

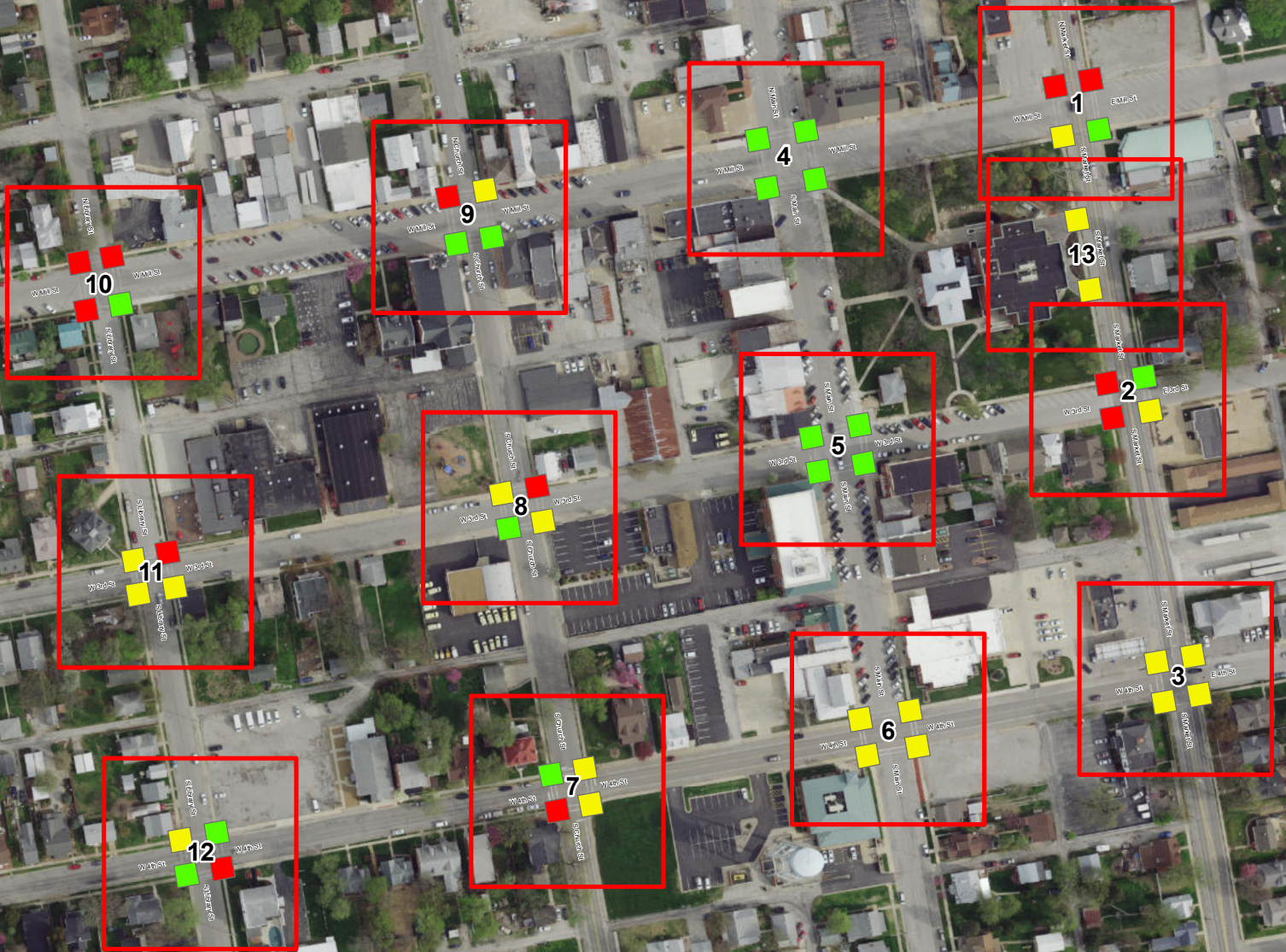
Review and Evaluation

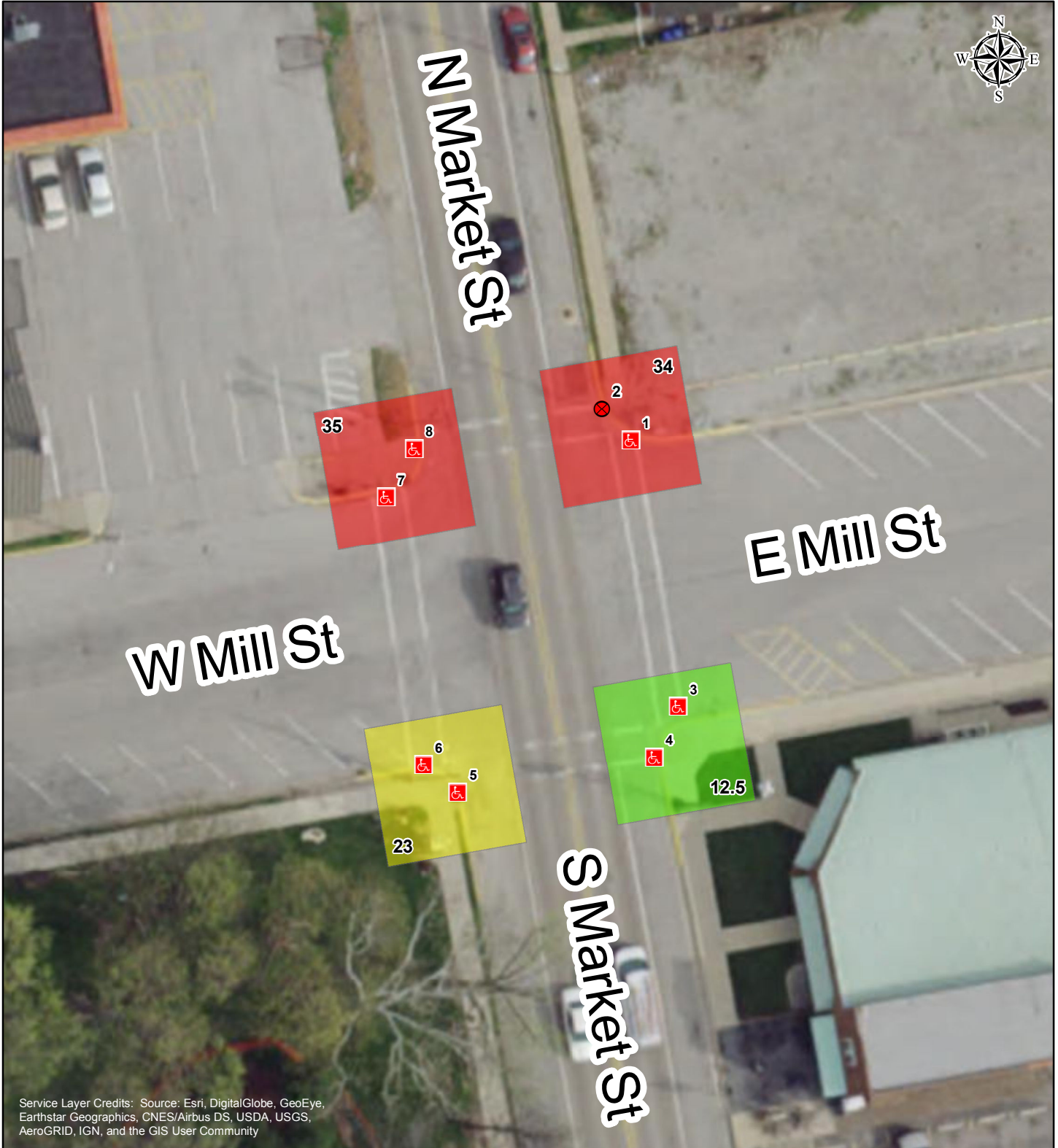
In January or February of each year, the Street Committee shall meet with the ADA Coordinator to review the City's efforts to comply with the ADA standards and to implement the foregoing plan. Progress shall be noted and the plan shall be evaluated for the purpose of determining its effectiveness. Modifications to the plan may be recommended by the Committee to the City Council, if deemed necessary. The Committee and the ADA Coordinator shall also review the prioritization of repairs, modifications or replacements of curbs and sidewalks for the upcoming year. They shall also review with the Council the sources and availability of funding to determine the number of ramps and curbs which can be brought into compliance with ADA standards.

ATTACHMENT A

1. Intersection Index
2. Quadrant Priority & Ramp Assessment

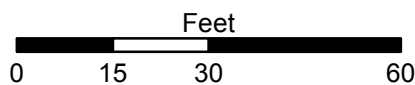
Intersection Index





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Map Date:
January 2018



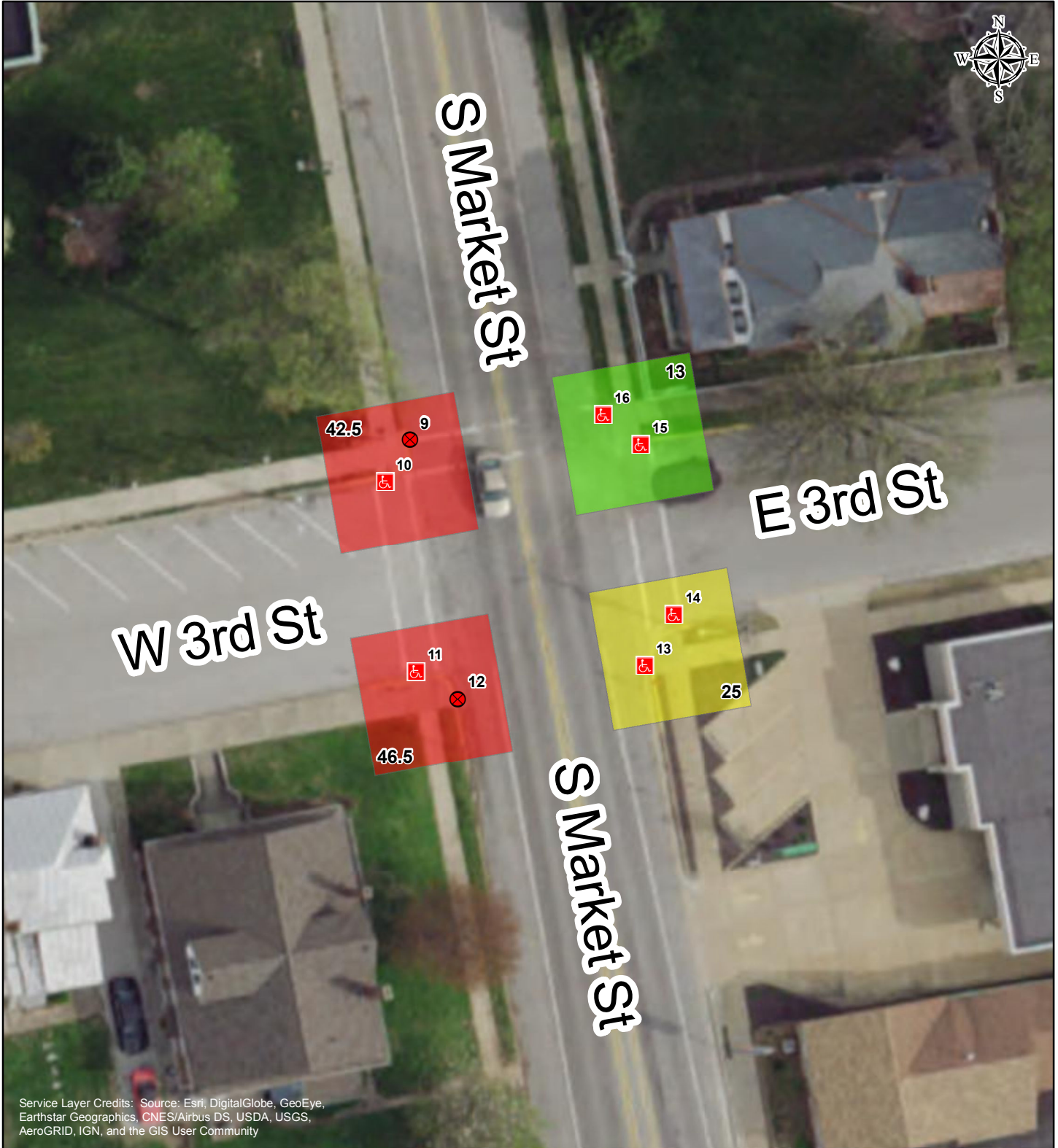
LEGEND	
Ramps	
	Compliant Ramp
	Non Compliant Ramp
	No Ramp
Quadrant Priority	
	Low
	Medium
	High

Intersection 1

Ramp Number	Detectable	Length	Slope	Width	Cross Slope	Landing Area	Landing Max Slope	Road Type	Stop Condition	Street Slope	Street Cross Slope	Condition	Inspection	Notes	Compliance
1	No	12.10	3.47	4.10	0.37	Exempt	0	Asphalt	Yes	3.95	7.80	Fair	11/27/2017 0:00		Non Compliant
2	NO RAMP														
3	No	10.80	7.45	15.00	0.77	No	6.37	Asphalt	Yes	2.55	3.13	Good	11/27/2017 0:00		Non Compliant
4	No	12.60	7.45	15.00	0.45	Yes	5.70	Asphalt	No	8.89	0.35	Good	11/27/2017 0:00		Non Compliant
5	No	13.80	9.82	9.00	0.01	Yes	6.78	Asphalt	No	0.82	4.45	Fair	11/27/2017 0:00		Non Compliant
6	No	18.30	0.82	9.00	8.63	Exempt	0	Asphalt	Yes	1.07	5.12	Fair	11/27/2017 0:00		Non Compliant
7	No	3.20	13.29	4.00	4.78	Yes	8.05	Asphalt	Yes	3.13	4.95	Bad	11/27/2017 0:00		Non Compliant
8	No	5.30	18.52	4.00	0.21	Yes	8.05	Asphalt	No	0.77	1.38	Bad	11/27/2017 0:00		Non Compliant

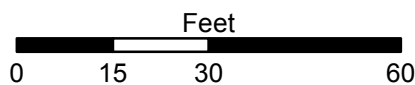


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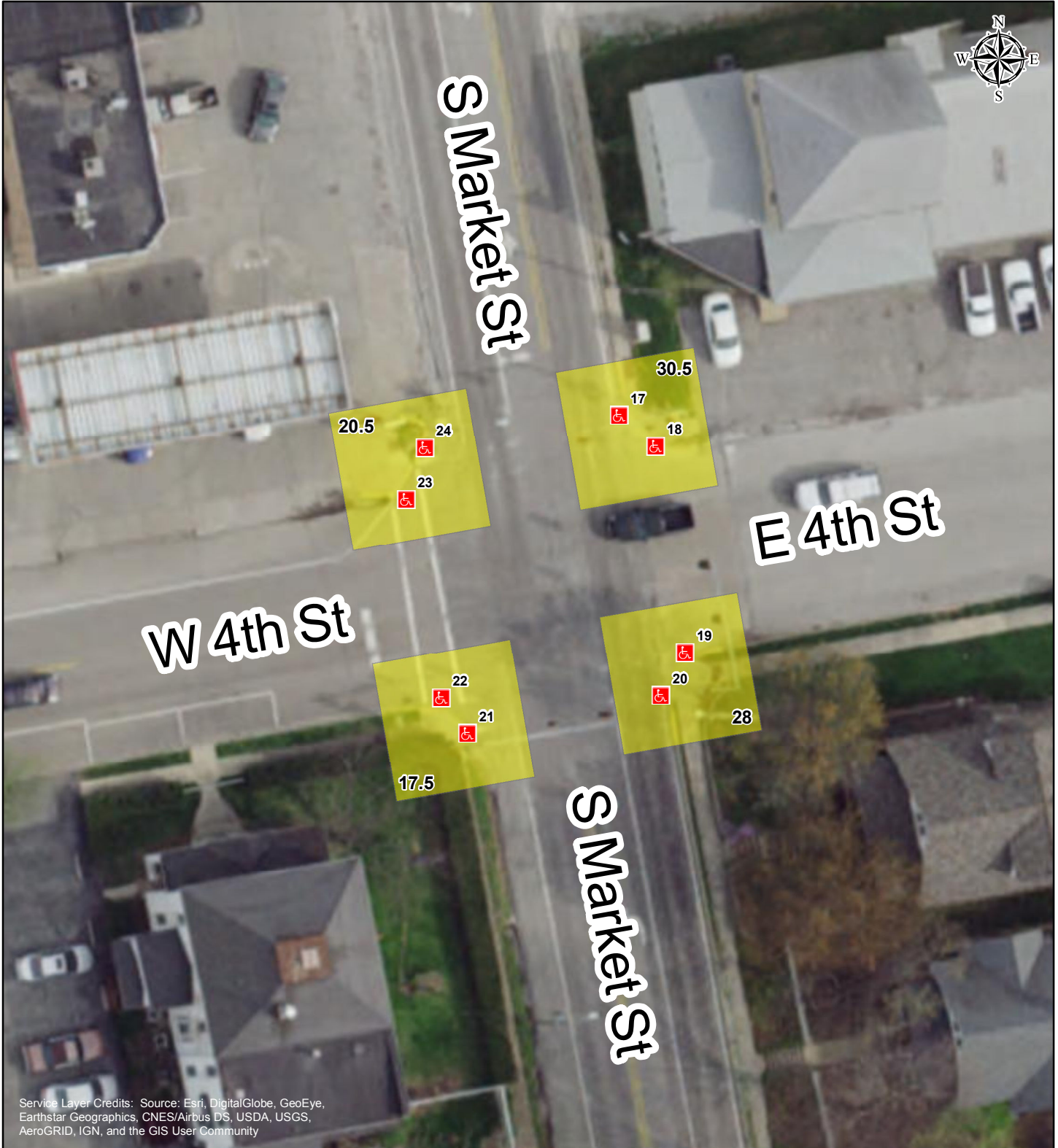
LEGEND	
Ramps	Quadrant Priority
Compliant Ramp	Low
Non Compliant Ramp	Medium
No Ramp	High

Intersection 2

Ramp Number	Detectable	Length	Slope	Width	Cross Slope	Landing Area	Landing Max Slope	Road Type	Stop Condition	Street Slope	Street Cross Slope	Condition	Inspection	Notes	Compliance
9										NO RAMP					
10	No	9.70	8.13	3.00	8.30	Yes	5.70	Asphalt	No	0.01	4.28	Good	11/27/2017 0:00		Non Compliant
11	No	7.20	11.87	3.40	5.03	Yes	5.03	Asphalt	No	1.30	5.87	Fair	11/27/2017 0:00		Non Compliant
12										NO RAMP					
13	No	6.70	6.95	5.00	11.29	Yes	5.03	Asphalt	No	21.14	4.87	Good	11/27/2017 0:00		Non Compliant
14	No	8.00	7.15	5.00	6.70	Yes	2.45	Asphalt	Yes	5.20	3.95	Good	11/27/2017 0:00		Non Compliant
15	No	3.60	4.62	4.00	1.32	Exempt	0	Asphalt	Yes	2.00	3.13	Fair	11/27/2017 0:00		Non Compliant
16	No	4.10	5.87	5.00	1.36	Yes	1.18	Asphalt	No	17.06	1.12	Fair	11/27/2017 0:00		Non Compliant



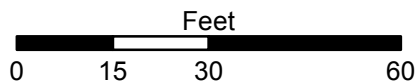
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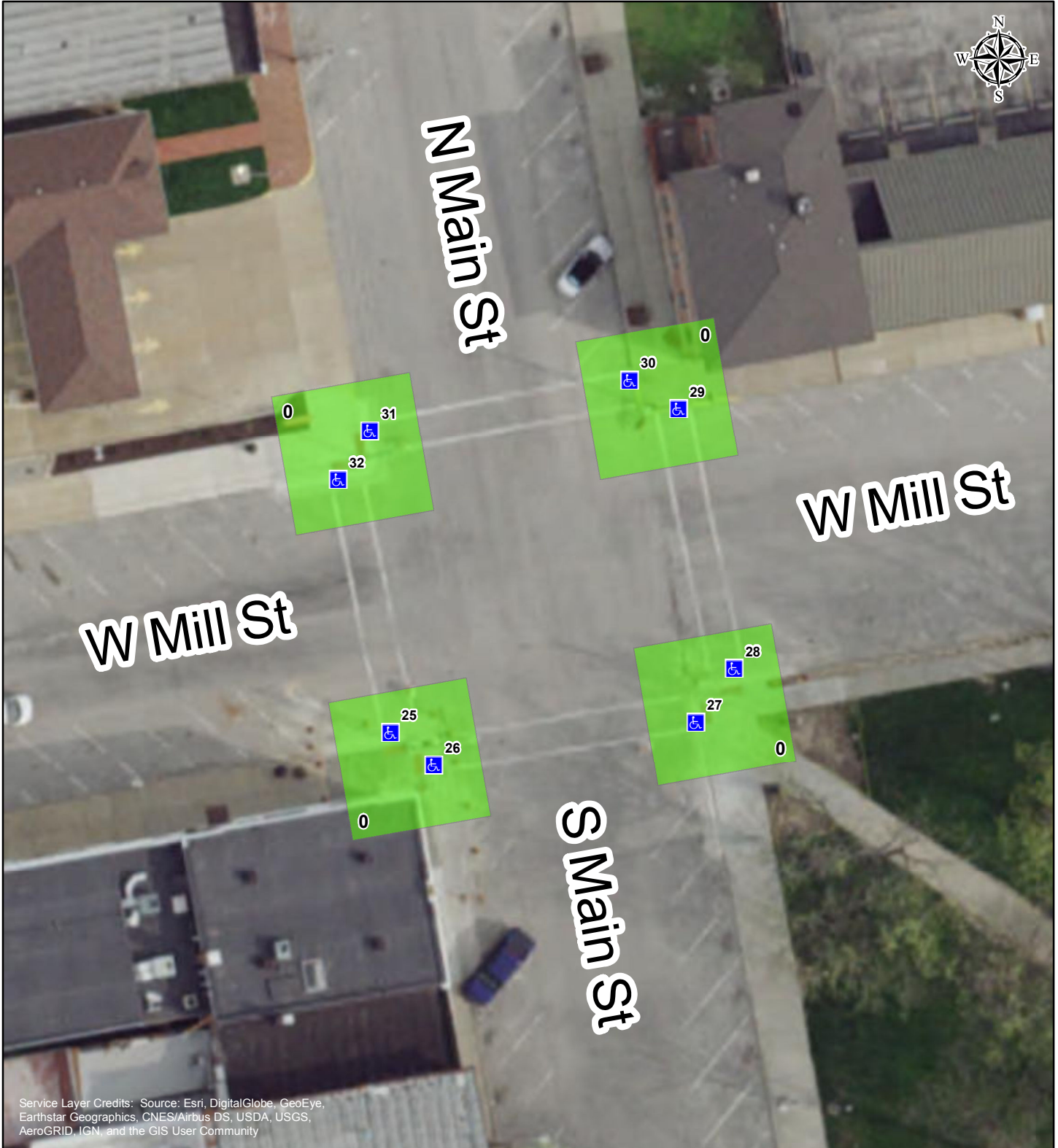
LEGEND

Ramps		Quadrant Priority	
	Compliant Ramp		Low
	Non Compliant Ramp		Medium
	No Ramp		High

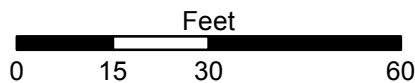
Intersection 3

Ramp Number	Detectable	Length	Slope	Width	Cross Slope	Landing Area	Landing Max Slope	Road Type	Stop Condition	Street Slope	Street Cross Slope	Condition	Inspection	Notes	Compliance
17	No	12.50	6.37	10.60	5.20	No	0	Asphalt	No	0.03	3.05	Fair	11/27/2017 0:00		Non Compliant
18	No	11.60	5.70	10.60	7.72	No	0	Concrete	No	5.00	2.88	Fair	11/27/2017 0:00		Non Compliant
19	No	12.90	0.67	7.00	13.99	Exempt	0	Concrete	No	5.95	8.63	Fair	11/27/2017 0:00		Non Compliant
20	No	12.50	13.29	7.00	0.87	Yes	3.95	Asphalt	No	0.63	4.62	Fair	11/27/2017 0:00		Non Compliant
21	No	7.80	5.60	11.30	1.83	Yes	3.30	Asphalt	No	0.08	5.03	Bad	11/27/2017 0:00		Non Compliant
22	No	9.40	1.05	11.30	3.05	Exempt	0	Asphalt	No	4.12	0.68	Bad	11/27/2017 0:00	HEDGES ON SIDEWALK	Non Compliant
23	No	11.60	1.15	7.40	4.12	Exempt	0	Asphalt	No	3.13	2.00	Fair	11/27/2017 0:00		Non Compliant
24	No	12.00	4.20	7.50	2.63	Exempt	0	Asphalt	No	2.88	4.28	Fair	11/27/2017 0:00	NEGATIVE SLOPE	Non Compliant

 Denotes Non-Compliance



Map Date:
January 2018



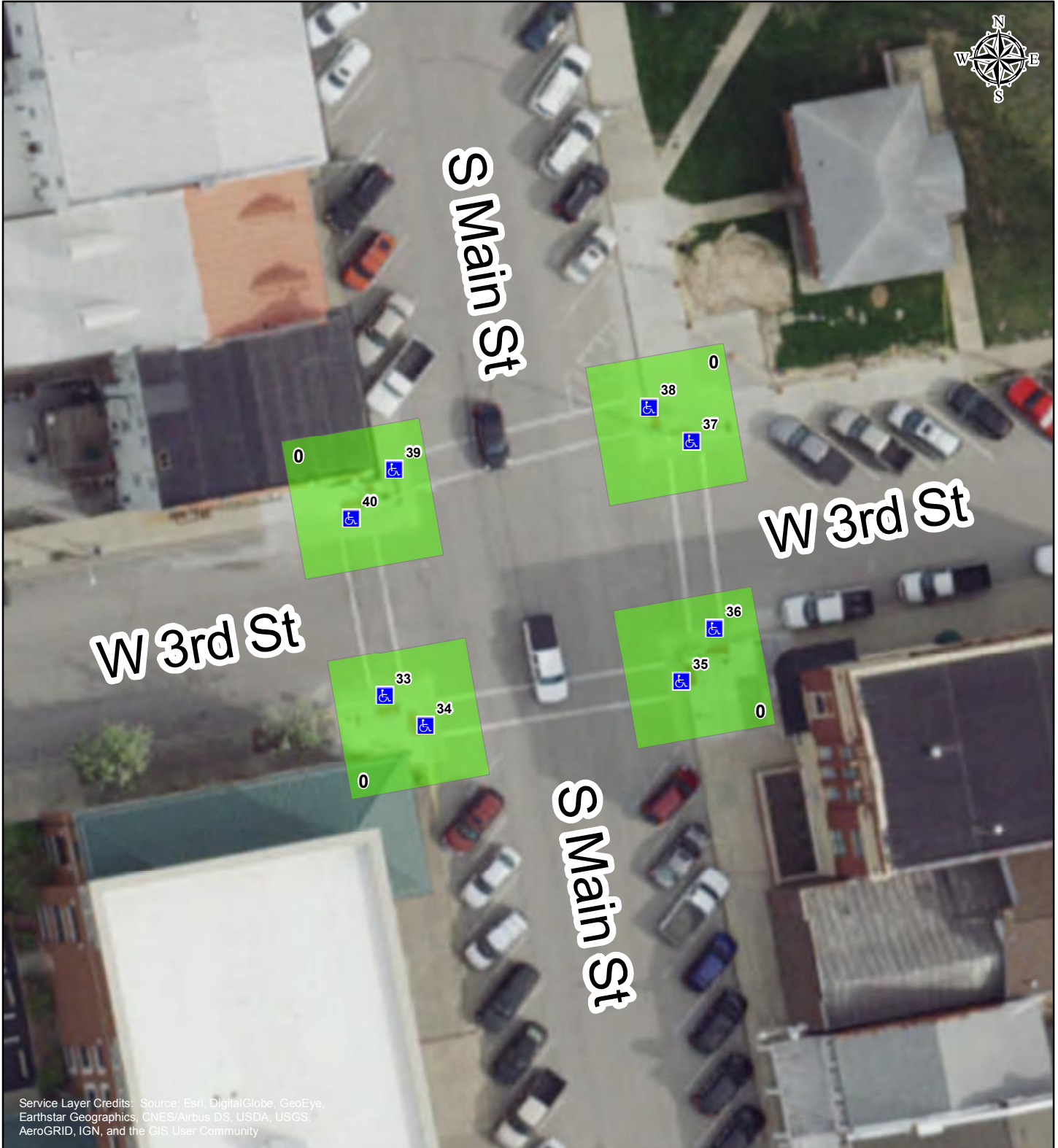
LEGEND	
Ramps	
	Compliant Ramp
	Non Compliant Ramp
	No Ramp
Quadrant Priority	
	Low
	Medium
	High

Intersection 4

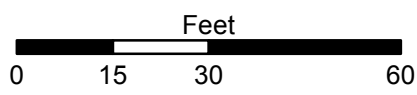
Ramp Number	Detectable	Length	Slope	Width	Cross Slope	Landing Area	Landing Max Slope	Road Type	Stop Condition	Street Slope	Street Cross Slope	Condition	Inspection	Notes	Compliance
25	Yes	2.60	5.87	5.00	0.83	Yes	1.22	Asphalt	Yes	2.16	4.45	Good	11/27/2017 0:00	BRICK XWALK	Compliant
26	Yes	3.00	2.97	5.00	1.26	Exempt	0	Asphalt	Yes	4.28	1.63	Good	11/27/2017 0:00	BRICK XWALK	Compliant
27	Yes	4.50	1.03	5.00	1.20	Exempt	0	Asphalt	Yes	2.08	2.45	Good	11/27/2017 0:00	BRICK XWALK	Compliant
28	Yes	6.90	4.45	5.00	1.05	Exempt	0	Asphalt	Yes	0.23	2.90	Good	11/27/2017 0:00	BRICK XWALK	Compliant
29	Yes	2.00	5.50	5.50	1.02	Yes	1.83	Asphalt	Yes	4.28	0.53	Good	11/27/2017 0:00	BRICK XWALK NEG SLOPE	Compliant
30	Yes	5.00	6.03	5.00	1.22	Yes	1.83	Asphalt	Yes	1.40	2.72	Good	11/27/2017 0:00	BRICK XWALK	Compliant
31	Yes	3.00	4.95	5.00	0.85	Exempt	0	Asphalt	Yes	4.87	4.03	Good	11/27/2017 0:00	BRICK XWALK NEG SLOPE	Compliant
32	Yes	2.50	0.45	5.00	1.50	Exempt	0	Asphalt	Yes	4.20	2.80	Good	11/27/2017 0:00	BRICK XWALK	Compliant



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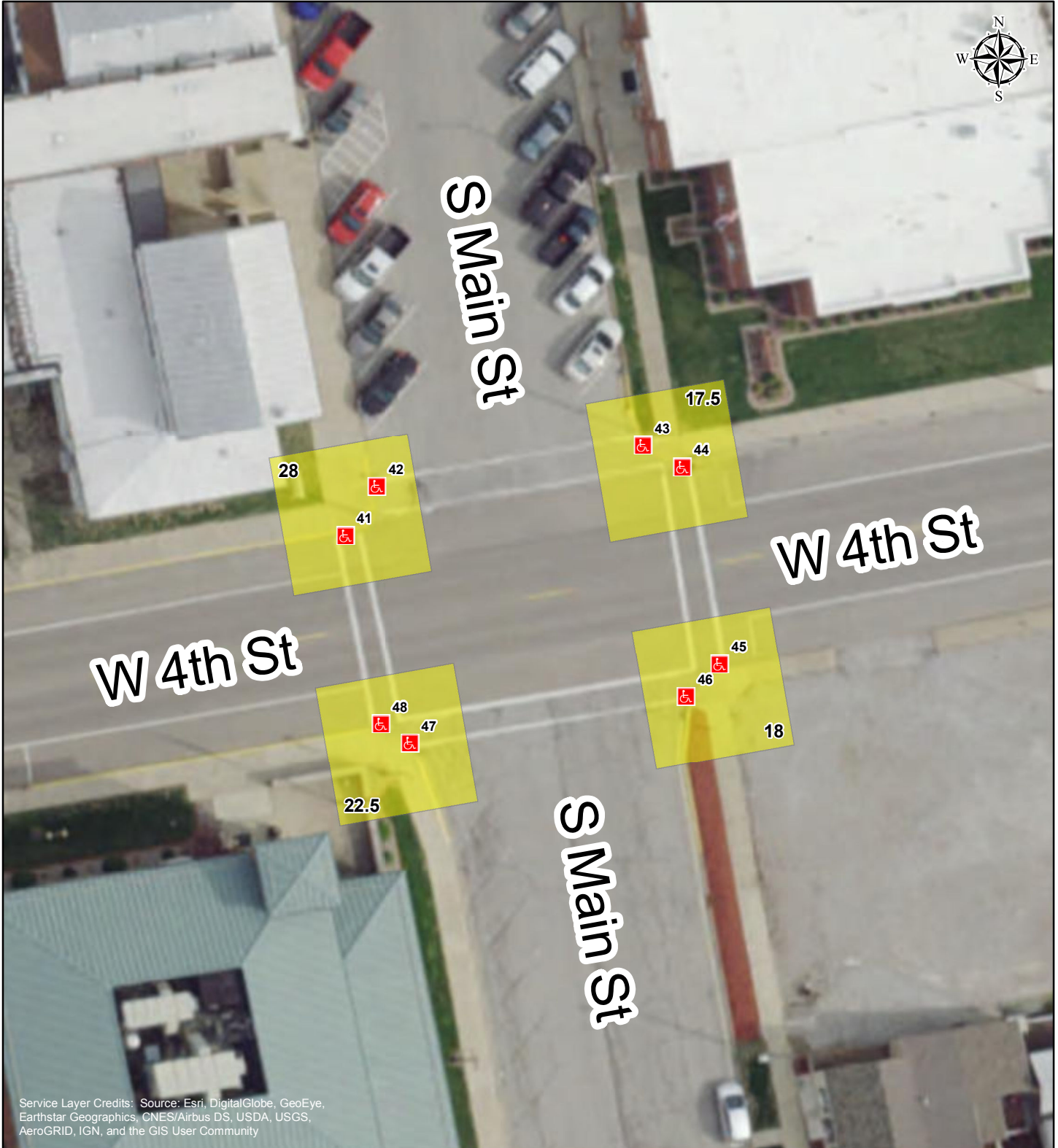
LEGEND	
Ramps	
	Compliant Ramp
	Non Compliant Ramp
	No Ramp
Quadrant Priority	
	Low
	Medium
	High

Intersection 5

Ramp Number	Detectable	Length	Slope	Width	Cross Slope	Landing Area	Landing Max Slope	Road Type	Stop Condition	Street Slope	Street Cross Slope	Condition	Inspection	Notes	Compliance
33	Yes	2.50	4.53	5.00	0.01	Exempt	0	Asphalt	Yes	1.60	2.63	Good	11/27/2017 0:00	BRICK XWALK	Compliant
34	Yes	4.00	0.85	5.00	1.10	Exempt	0	Asphalt	Yes	8.13	0.85	Good	11/27/2017 0:00	BRICK XWALK	Compliant
35	Yes	5.00	4.20	5.00	1.26	Exempt	0	Asphalt	Yes	4.28	0.08	Good	11/27/2017 0:00	BRICK XWALK	Compliant
36	Yes	2.50	6.03	5.00	1.12	Yes	1.70	Asphalt	Yes	5.12	1.03	Good	11/27/2017 0:00	BRICK XWALK	Compliant
37	Yes	3.00	2.97	5.00	0.35	Exempt	0	Asphalt	Yes	4.45	0.08	Good	11/27/2017 0:00	BRICK XWALK	Compliant
38	Yes	3.00	4.70	5.00	1.10	Exempt	0	Asphalt	Yes	1.91	0.11	Good	11/27/2017 0:00	BRICK XWALK	Compliant
39	Yes	2.90	0.39	5.00	1.05	Exempt	0	Asphalt	Yes	3.57	0.06	Good	11/27/2017 0:00	BRICK XWALK	Compliant
40	Yes	3.00	4.30	5.00	1.20	Exempt	0	Asphalt	Yes	1.83	1.83	Good	11/27/2017 0:00	BRICK XWALK	Compliant

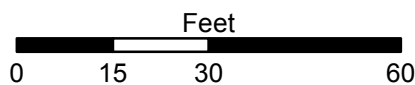


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LEGEND	
Ramps	Quadrant Priority
Compliant Ramp	Low
Non Compliant Ramp	Medium
No Ramp	High

Intersection 6

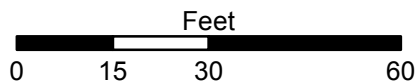
Ramp Number	Detectable	Length	Slope	Width	Cross Slope	Landing Area	Landing Max Slope	Road Type	Stop Condition	Street Slope	Street Cross Slope	Condition	Inspection	Notes	Compliance
41	No	6.00	15.49	4.00	0.31	Yes	2.45	Asphalt	No	1.15	0.00	Fair	11/27/2017 0:00		Non Compliant
42	No	4.80	12.46	4.00	2.45	Yes	2.80	Asphalt	Yes	1.03	1.61	Fair	11/27/2017 0:00		Non Compliant
43	No	6.80	0.58	9.50	8.89	Exempt	0	Asphalt	Yes	4.20	3.13	Good	11/28/2017 0:00		Non Compliant
44	No	5.50	8.05	9.50	0.13	Yes	4.12	Asphalt	No	0.87	2.16	Good	11/28/2017 0:00		Non Compliant
45	No	7.70	2.97	9.70	2.72	Exempt	0	Asphalt	No	8.22	1.08	Fair	11/28/2017 0:00		Non Compliant
46	No	6.50	6.78	9.70	2.72	Yes	5.20	Asphalt	Yes	1.91	8.80	Fair	11/28/2017 0:00		Non Compliant
47	No	10.90	7.45	9.90	5.30	Yes	7.63	Asphalt	Yes	1.83	11.87	Good	11/28/2017 0:00		Non Compliant
48	No	10.00	2.88	9.90	7.80	Exempt	0	Asphalt	No	7.15	0.16	Good	11/28/2017 0:00		Non Compliant

 Denotes Non-Compliance



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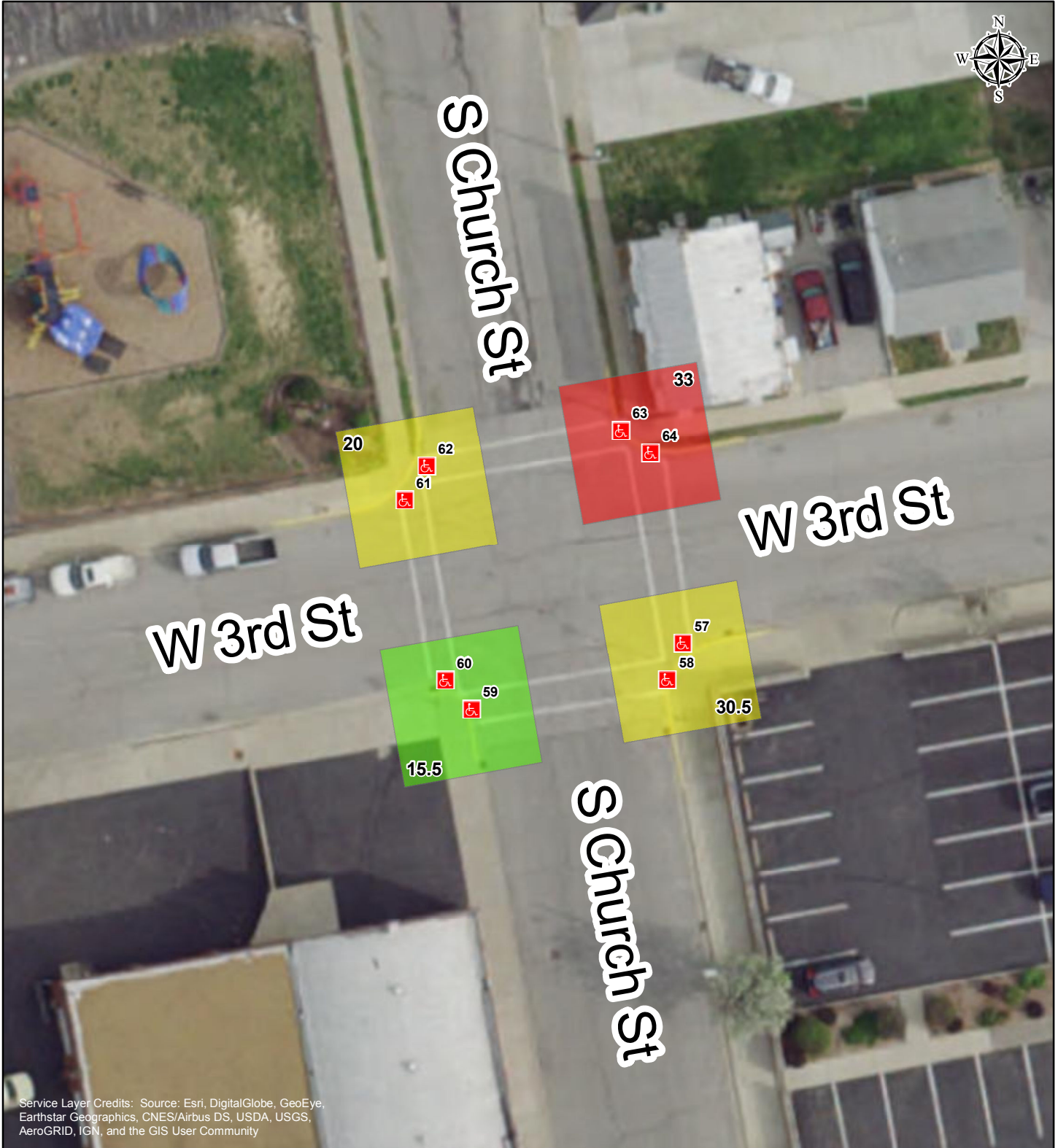
LEGEND	
Ramps	Quadrant Priority
Compliant Ramp	Low
Non Compliant Ramp	Medium
No Ramp	High

Intersection 7

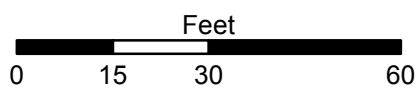
Ramp Number	Detectable	Length	Slope	Width	Cross Slope	Landing Area	Landing Max Slope	Road Type	Stop Condition	Street Slope	Street Cross Slope	Condition	Inspection	Notes	Compliance
49	No	9.40	1.91	8.00	5.40	Exempt	0	Asphalt	Yes	0.85	1.82	Fair	11/28/2017 0:00		Non Compliant
50	No	7.90	8.63	8.00	3.77	Yes	5.70	Asphalt	Yes	1.91	1.63	Fair	11/28/2017 0:00		Non Compliant
51	NO RAMP														
52	No	2.30	14.09	4.60	9.74	Yes	6.45	Asphalt	Yes	0.49	4.87	Bad	11/28/2017 0:00		Non Compliant
53	No	5.80	3.87	7.30	0.93	Exempt	0	Asphalt	Yes	8.22	8.30	Good	11/28/2017 0:00		Non Compliant
54	No	5.80	0.55	7.80	4.62	Exempt	0	Asphalt	Yes	10.07	4.28	Good	11/28/2017 0:00		Non Compliant
55	No	5.70	9.57	6.90	0.93	Yes	6.12	Asphalt	Yes	1.22	7.05	Fair	11/28/2017 0:00		Non Compliant
56	No	5.40	0.88	6.90	10.07	Exempt	0	Asphalt	Yes	8.13	1.65	Fair	11/28/2017 0:00		Non Compliant



Denotes Non-Compliance



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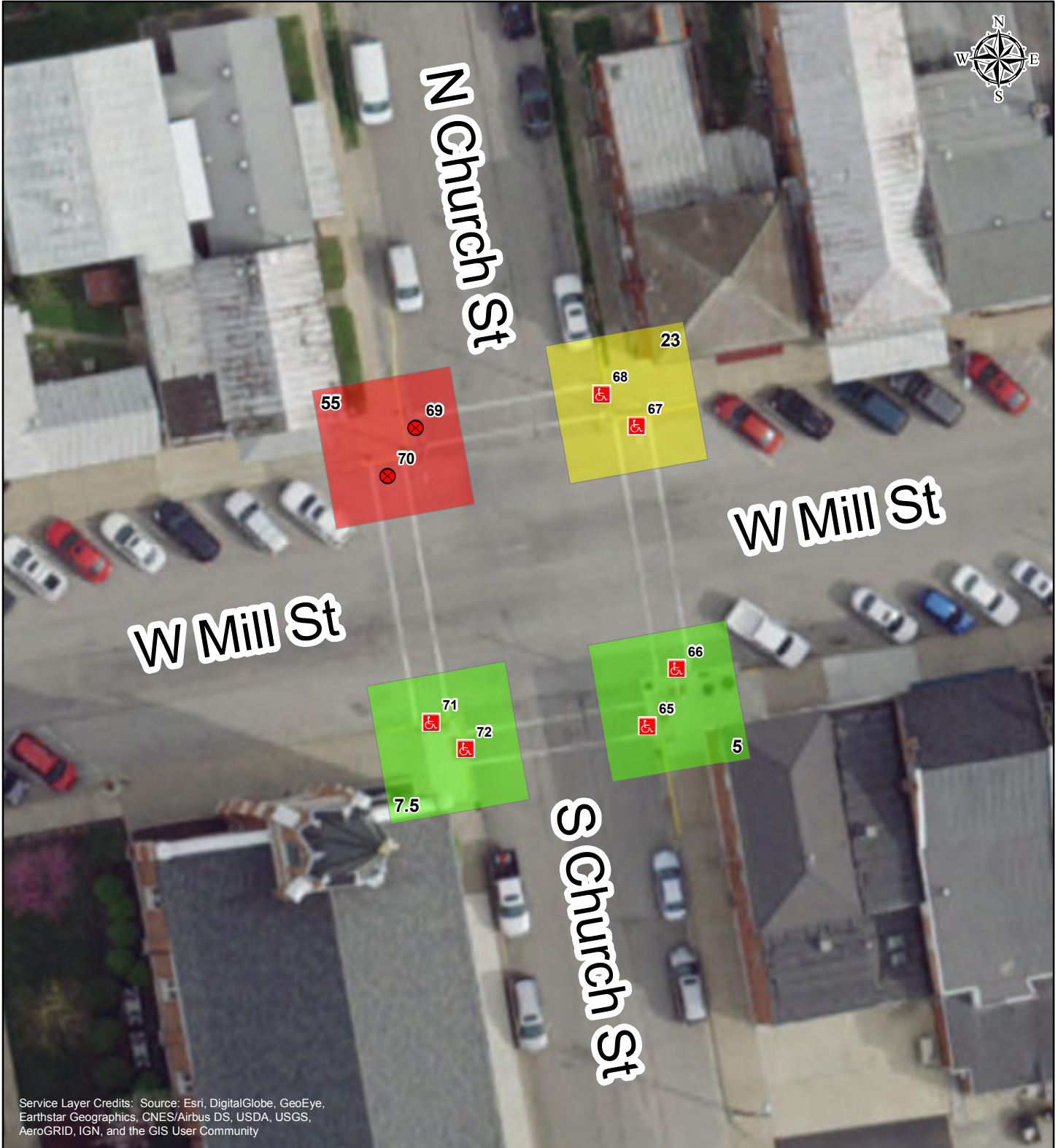


LEGEND	
Ramps	
	Compliant Ramp
	Non Compliant Ramp
	No Ramp
Quadrant Priority	
	Low
	Medium
	High

Intersection 8

Ramp Number	Detectable	Length	Slope	Width	Cross Slope	Landing Area	Landing Max Slope	Road Type	Stop Condition	Street Slope	Street Cross Slope	Condition	Inspection	Notes	Compliance
57	No	10.00	5.70	10.20	10.41	Yes	6.03	Asphalt	Yes	1.91	2.35	Fair	11/28/2017 0:00		Non Compliant
58	No	10.40	10.99	10.20	4.95	Yes	6.12	Asphalt	Yes	0.08	1.63	Fair	11/28/2017 0:00		Non Compliant
59	No	6.80	3.47	12.80	0.85	Exempt	0	Asphalt	Yes	8.47	0.68	Fair	11/28/2017 0:00		Non Compliant
60	No	10.70	2.63	12.60	3.05	Exempt	0	Asphalt	Yes	2.80	4.37	Fair	11/28/2017 0:00		Non Compliant
61	No	8.10	9.09	6.00	1.03	Yes	3.60	Asphalt	Yes	3.13	6.12	Good	11/28/2017 0:00		Non Compliant
62	No	8.20	3.30	6.00	9.49	Exempt	0	Asphalt	Yes	9.19	1.83	Good	11/28/2017 0:00		Non Compliant
63	No	10.10	11.49	5.00	7.88	Yes	7.25	Asphalt	Yes	2.08	1.42	Fair	11/28/2017 0:00		Non Compliant
64	No	6.50	6.78	5.00	10.16	Yes	8.72	Asphalt	Yes	2.16	5.03	Fair	11/28/2017 0:00		Non Compliant

 Denotes Non-Compliance

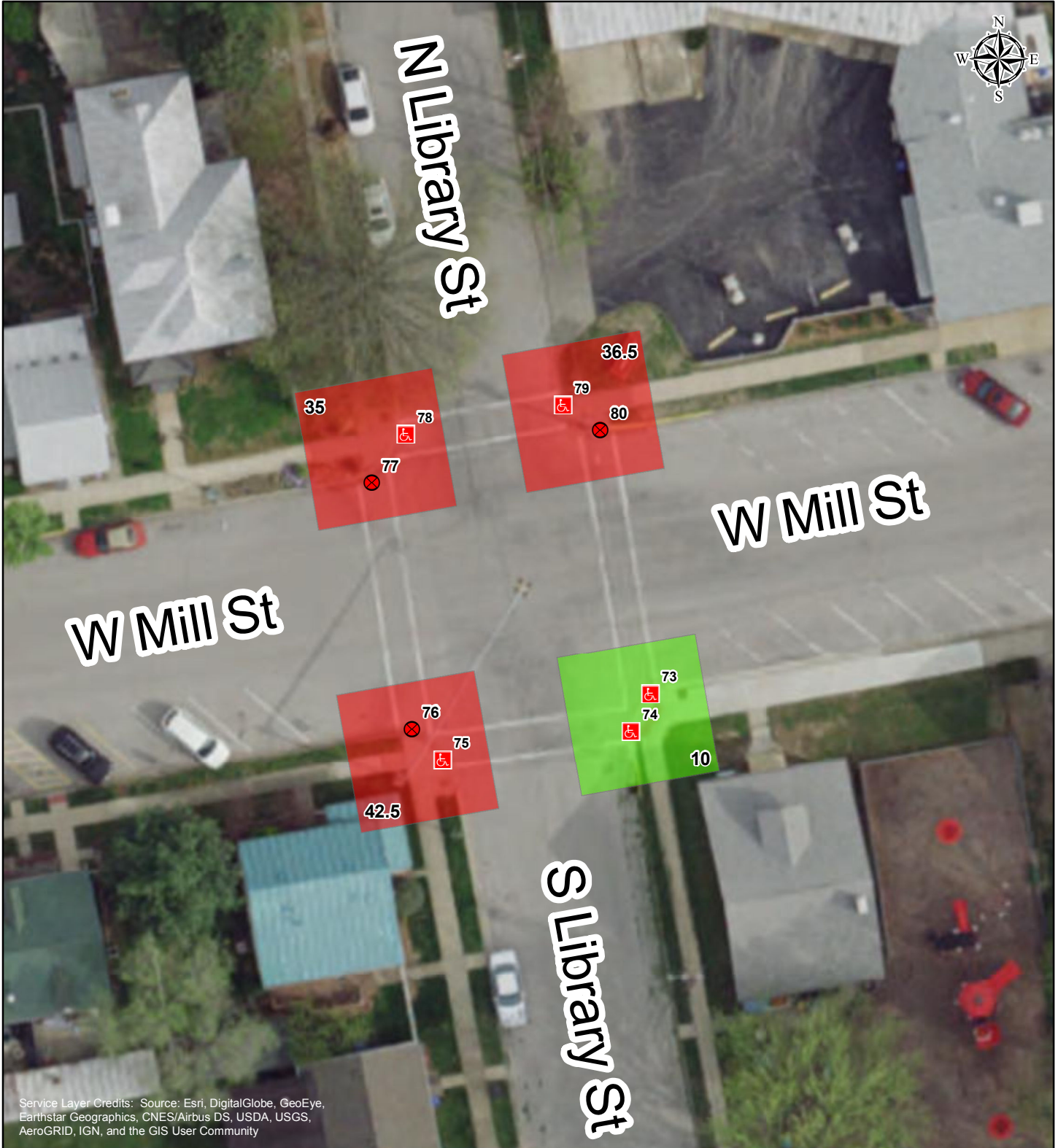


Intersection 9

Ramp Number	Detectable	Length	Slope	Width	Cross Slope	Landing Area	Landing Max Slope	Road Type	Stop Condition	Street Slope	Street Cross Slope	Condition	Inspection	Notes	Compliance
65	Yes	17.50	9.74	4.00	1.00	Yes	10.16	Asphalt	Yes	2.35	1.42	Good	11/28/2017 0:00		Non Compliant
66	Yes	14.50	5.12	4.00	3.10	Yes	8.55	Asphalt	Yes	2.80	3.22	Good	11/28/2017 0:00		Non Compliant
67	No	11.00	2.88	4.00	2.72	Exempt	0.00	Asphalt	Yes	1.12	0.25	Fair	11/28/2017 0:00		Non Compliant
68	No	7.00	14.84	4.00	0.95	Yes	7.63	Asphalt	Yes	0.16	3.67	Fair	11/28/2017 0:00		Non Compliant
69	NO RAMP														
70	NO RAMP														
71	Yes	9.00	1.12	4.00	3.13	Exempt	0	Asphalt	Yes	0.95	2.35	Good	11/28/2017 0:00		Non Compliant
72	Yes	7.70	5.40	5.00	5.03	Yes	4.03	Asphalt	Yes	4.53	1.91	Good	11/28/2017 0:00		Non Compliant



Denotes Non-Compliance

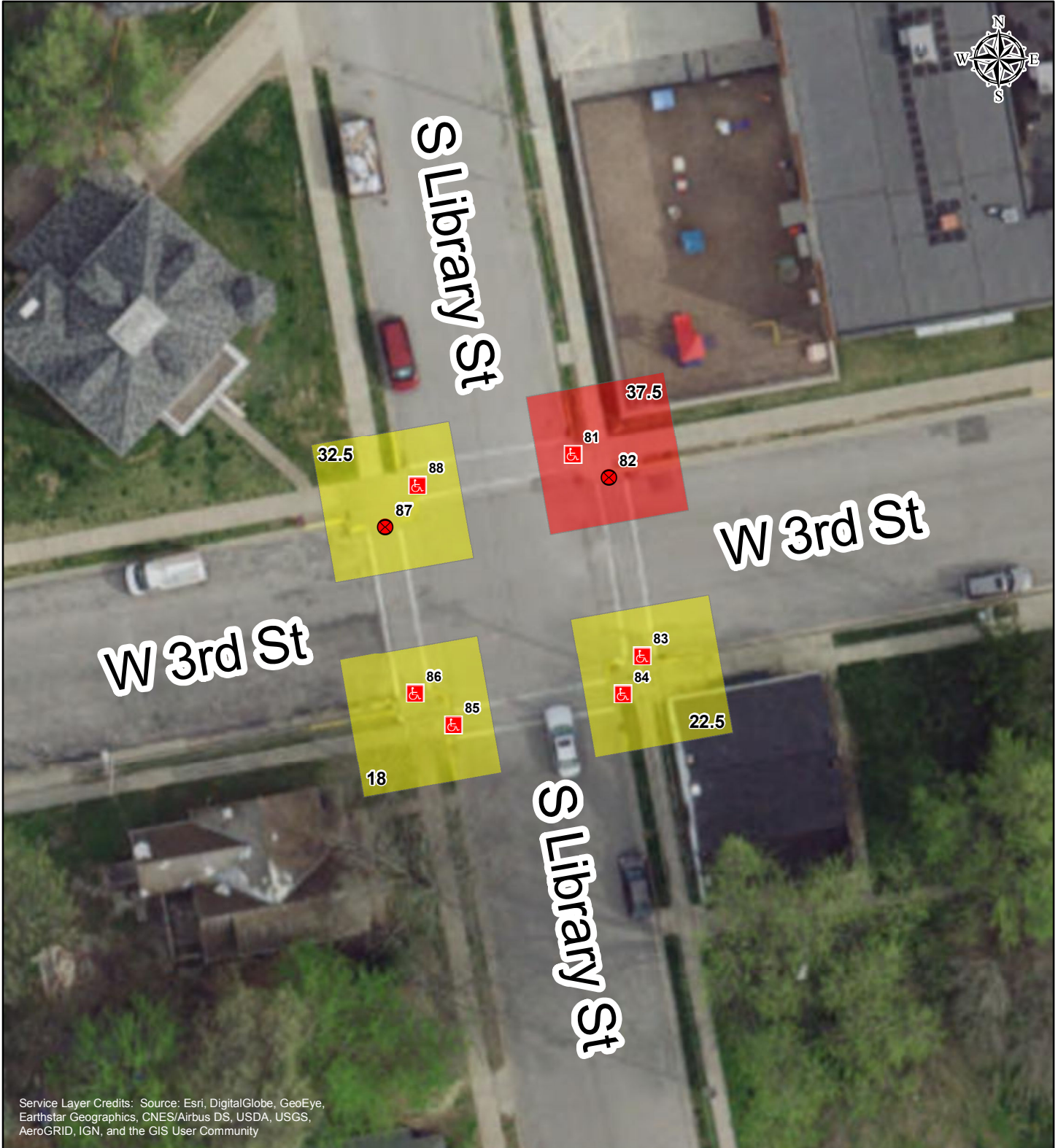


Intersection 10

Ramp Number	Detectable	Length	Slope	Width	Cross Slope	Landing Area	Landing Max Slope	Road Type	Stop Condition	Street Slope	Street Cross Slope	Condition	Inspection	Notes	Compliance
73	Yes	14.70	0.87	8.50	5.50	Exempt	0	Asphalt	Yes	1.75	3.05	Good	11/28/2017 0:00		Non Compliant
74	Yes	8.60	1.40	8.50	5.20	Exempt	0	Asphalt	Yes	2.25	2.45	Good	11/28/2017 0:00		Non Compliant
75	No	6.80	1.91	10.30	9.57	Exempt	0	Asphalt	Yes	2.97	0.62	Bad	11/28/2017 0:00		Non Compliant
76	NO RAMP														
77	NO RAMP														
78	No	7.70	2.72	5.00	1.12	Exempt	0	Asphalt	Yes	5.40	5.12	Bad	11/28/2017 0:00		Non Compliant
79	No	9.10	9.57	5.30	1.30	Yes	2.97	Asphalt	Yes	3.38	3.57	Fair	11/28/2017 0:00		Non Compliant
80	NO RAMP														



Denotes Non-Compliance

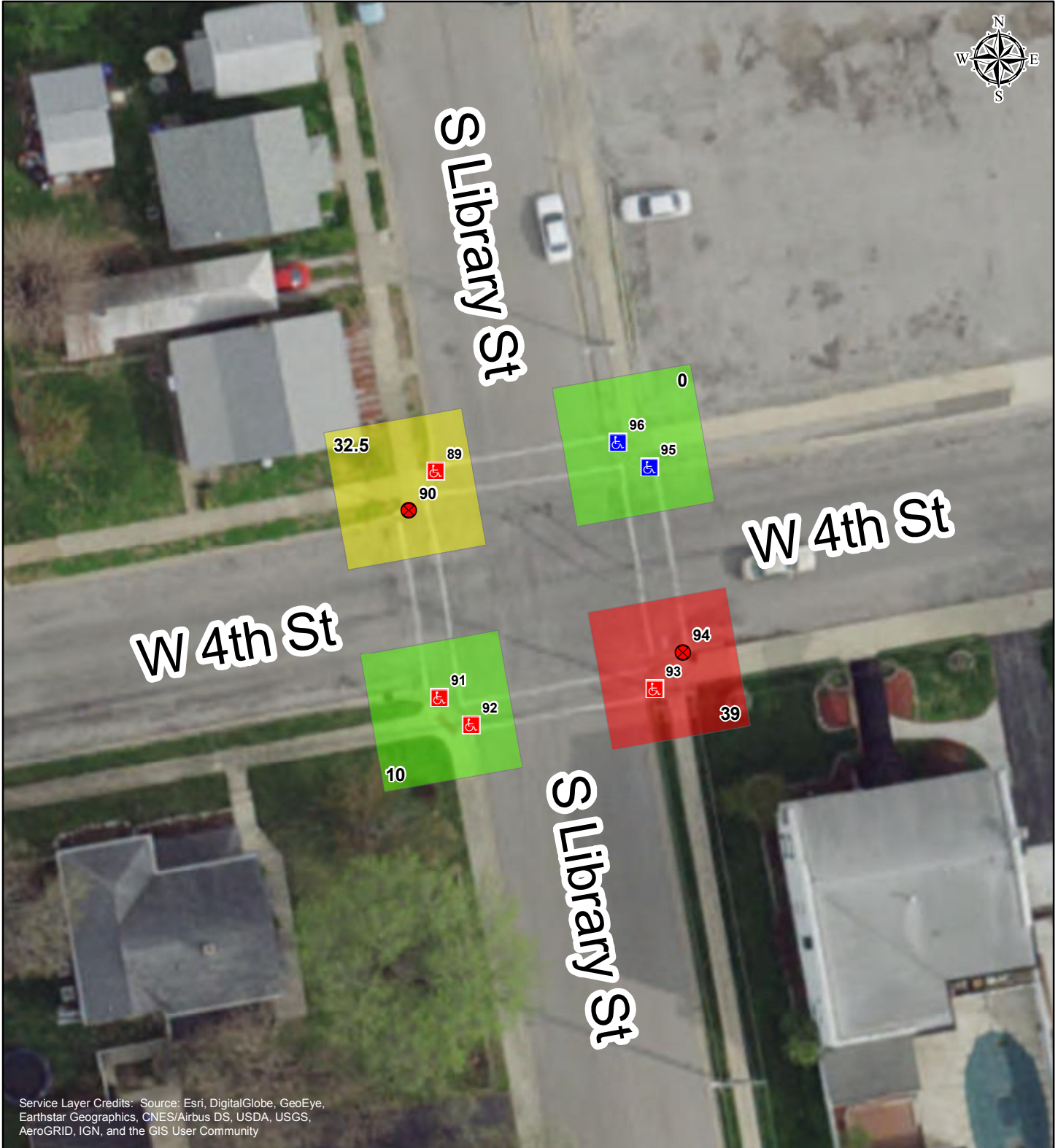


Intersection 11

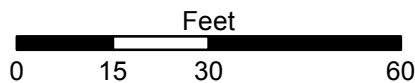
Ramp Number	Detectable	Length	Slope	Width	Cross Slope	Landing Area	Landing Max Slope	Road Type	Stop Condition	Street Slope	Street Cross Slope	Condition	Inspection	Notes	Compliance
81	No	8.60	8.40	7.70	3.47	Yes	6.12	Asphalt	Yes	3.57	2.08	Good	11/28/2017 0:00		Non Compliant
82	NO RAMP														
83	No	10.60	6.78	6.80	4.95	No	0.00	Asphalt	Yes	0.08	2.45	Bad	11/28/2017 0:00	HOLE NEXT TO RAMP BY INLET	Non Compliant
84	No	11.80	5.20	5.50	2.00	Yes	4.70	Asphalt	Yes	0.00	1.20	Bad	11/28/2017 0:00		Non Compliant
85	No	4.50	3.87	5.40	0.25	Exempt	0	Asphalt	Yes	14.09	3.05	Fair	11/28/2017 0:00		Non Compliant
86	No	4.80	7.88	5.70	5.03	Yes	5.30	Asphalt	Yes	6.62	0.95	Fair	11/28/2017 0:00	DEEP DITCH NEXT TO RAMP	Non Compliant
87	NO RAMP														
88	No	5.40	0.53	5.50	0.53	Exempt	0	Asphalt	Yes	9.91	2.97	Good	11/28/2017 0:00		Non Compliant



Denotes Non-Compliance



Service Layer Credits: Source: Esri, DigitalGlobe, GeoEye, Earthstar Geographics, CNES/Airbus DS, USDA, USGS, AeroGRID, IGN, and the GIS User Community



LEGEND

Ramps		Quadrant Priority	
	Compliant Ramp		Low
	Non Compliant Ramp		Medium
	No Ramp		High

Intersection 12

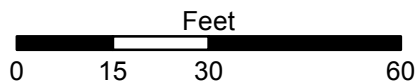
Ramp Number	Detectable	Length	Slope	Width	Cross Slope	Landing Area	Landing Max Slope	Road Type	Stop Condition	Street Slope	Street Cross Slope	Condition	Inspection	Notes	Compliance
89	No	5.70	7.05	6.00	0.53	Yes	5.70	Asphalt	Yes	3.77	5.78	Good	11/28/2017 0:00		Non Compliant
90	NO RAMP														
91	Yes	8.10	8.38	6.00	0.53	Yes	4.95	Asphalt	Yes	4.28	2.00	Good	11/28/2017 0:00		Non Compliant
92	Yes	6.60	0.53	6.00	7.55	Exempt	0	Asphalt	Yes	7.15	0.70	Good	11/28/2017 0:00		Non Compliant
93	No	4.70	6.62	5.50	5.60	Yes	2.88	Asphalt	Yes	2.35	5.78	Fair	11/28/2017 0:00		Non Compliant
94	NO RAMP														
95	Yes	2.70	1.12	11.70	2.00	Exempt	0	Asphalt	Yes	12.96	3.95	Good	11/28/2017 0:00		Compliant
96	Yes	2.60	1.58	11.70	1.20	Exempt	0	Asphalt	Yes	4.53	3.95	Good	11/28/2017 0:00		Compliant



Denotes Non-Compliance



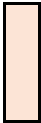
Map Date:
January 2018
HMG
Engineers • Surveyors



LEGEND	
Ramps	Quadrant Priority
Compliant Ramp	Low
Non Compliant Ramp	Medium
No Ramp	High

Intersection 13

Ramp Number	Detectable	Length	Slope	Width	Cross Slope	Landing Area	Landing Max Slope	Road Type	Stop Condition	Street Slope	Street Cross Slope	Condition	Inspection	Notes	Compliance
97	No	15.80	6.53	5.40	5.03	Yes	3.57	Concrete	No	4.87	2.88	Fair	11/28/2017 0:00	TRIP HAZARD	Non Compliant
98	No	13.00	8.47	3.90	4.70	Yes	3.67	Concrete	No	0.68	2.45	Good	11/28/2017 0:00		Non Compliant
99	No	13.00	9.74	3.60	1.60	Yes	2.45	Concrete	No	4.28	2.97	Fair	11/28/2017 0:00		Non Compliant
100	No	15.00	7.80	5.00	5.30	Yes	4.87	Concrete	No	0.82	1.63	Good	11/28/2017 0:00		Non Compliant



Denotes Non-Compliance

ATTACHMENT B

1. U.S. Department of Transportation Federal Highway Administration (USDOT FHWA) Design Specifications for Curb Ramps
2. Illinois Department of Transportation (IDOT) Standards
 - A) 424001-10 : Perpendicular Curb Ramps for Sidewalk
 - B) 424006-03 : Diagonal Curb Ramps for Sidewalks
 - C) 424011-03 : Corner Parallel Curb Ramps for Sidewalk
 - D) 424016-04 : Mid-Block Ramps for Sidewalks
 - E) 424021-04 : Depressed Corner for Sidewalks
 - F) 424026-02 : Entrance/Alley Pedestrian Crossings
 - G) 424031-01 : Median Pedestrian Crossings

U.S. DEPARTMENT OF TRANSPORTATION FEDERAL HIGHWAY ADMINISTRATION DESIGN SPECIFICATIONS FOR CURB RAMPS

PERPENDICULAR CURB RAMPS

Ramp Slope = 7.1 +/- 1.2 percent

Gutter Slope = 5 percent maximum

Changes in Level = Flush

Ramp Width = 48 in. recommended minimum

Landing Width = 48 in. recommended minimum

Flare Slope = 10 percent maximum

Cross Slope = 2 percent maximum

Truncated Domes = 24 in.

PARALLEL AND COMBINATION CURB RAMPS

Parallel Ramp Slope = 7.1 percent

Gutter Slope = 5 percent maximum

Changes in Level = None

Ramp Width = 48 in. recommended minimum

Landing Width = 48 in. recommended minimum

Landing Slope = 2 percent maximum towards the gutter

Cross Slope = 2 percent maximum

Truncated Domes = 24 in.

DIAGONAL CURB RAMPS

Ramp Slope = 8.33 percent

Gutter Slope = 2.0 percent maximum

Changes in Level = None

Ramp Width = 48 in. recommended minimum

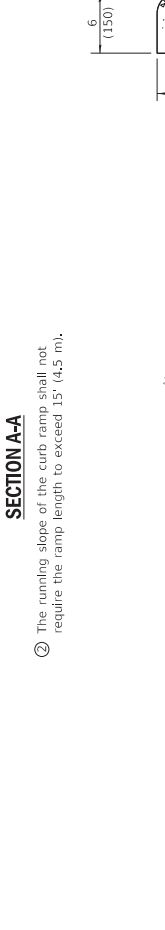
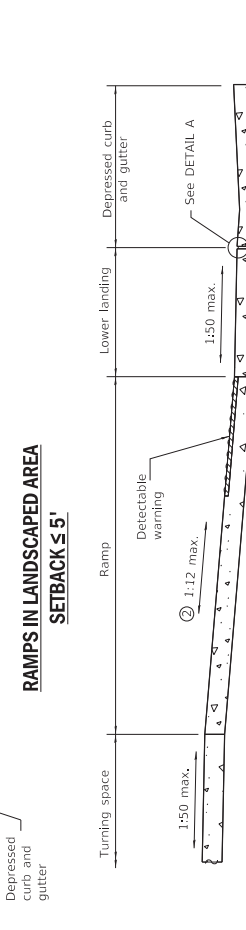
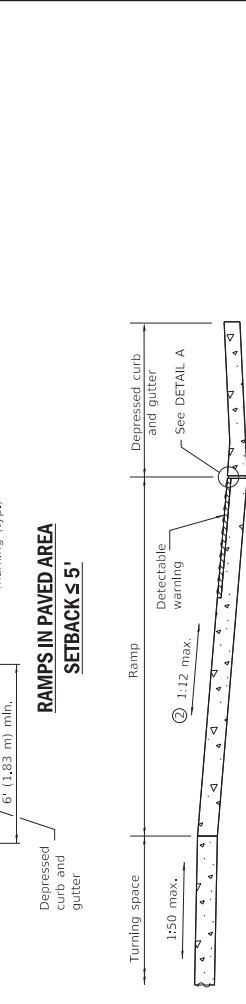
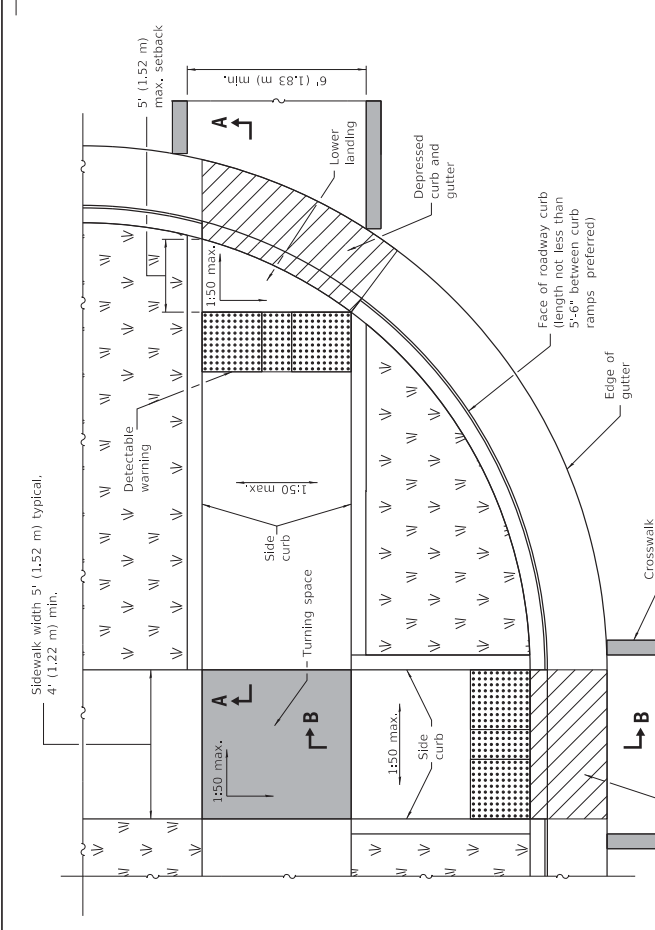
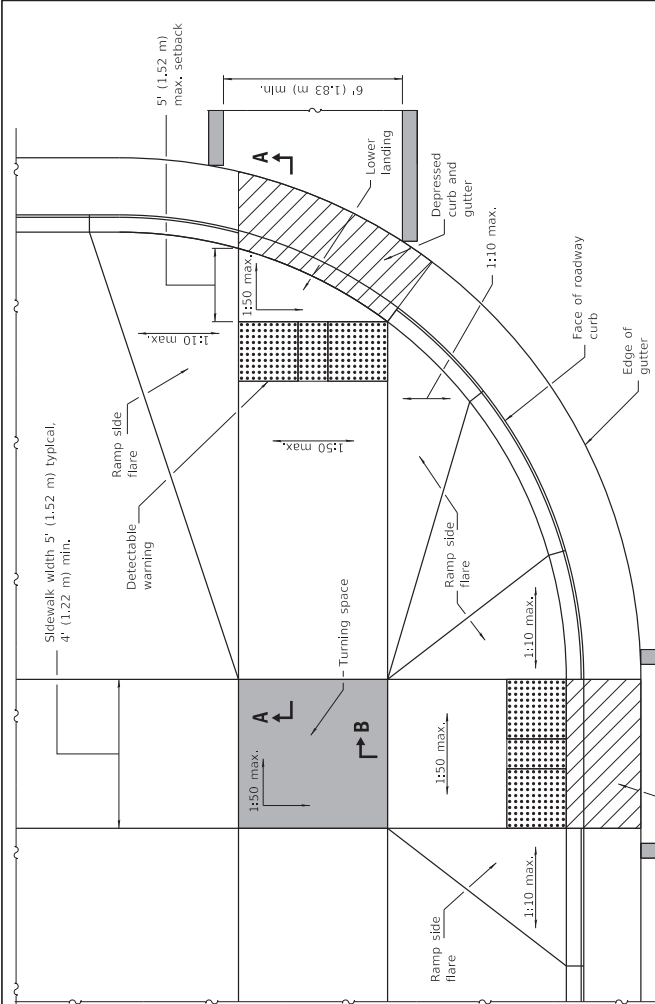
Landing Width = 48 in. recommended minimum

Flare Slope = 10 percent maximum

Cross Slope = 2 percent maximum

Truncated Domes = 24 in.

Clear Space = 48 in. minimum



See Sheet 2 for GENERAL NOTES.

DATE	REVISIONS
1-1-18	Omitted diagonal slope at turning spaces and lower landings.
1-1-17	Added 2" dimension to det. warnings for setbacks greater than 5'.

PERPENDICULAR CURB RAMPS FOR SIDEWALKS (Sheet 1 of 2)

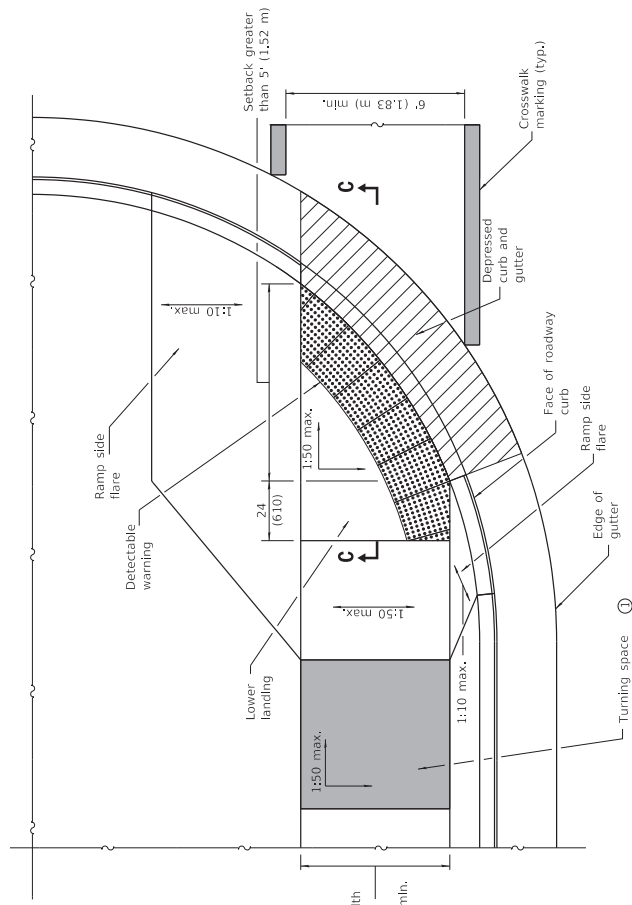
STANDARD 424001-10

ILLINOIS DEPARTMENT OF TRANSPORTATION

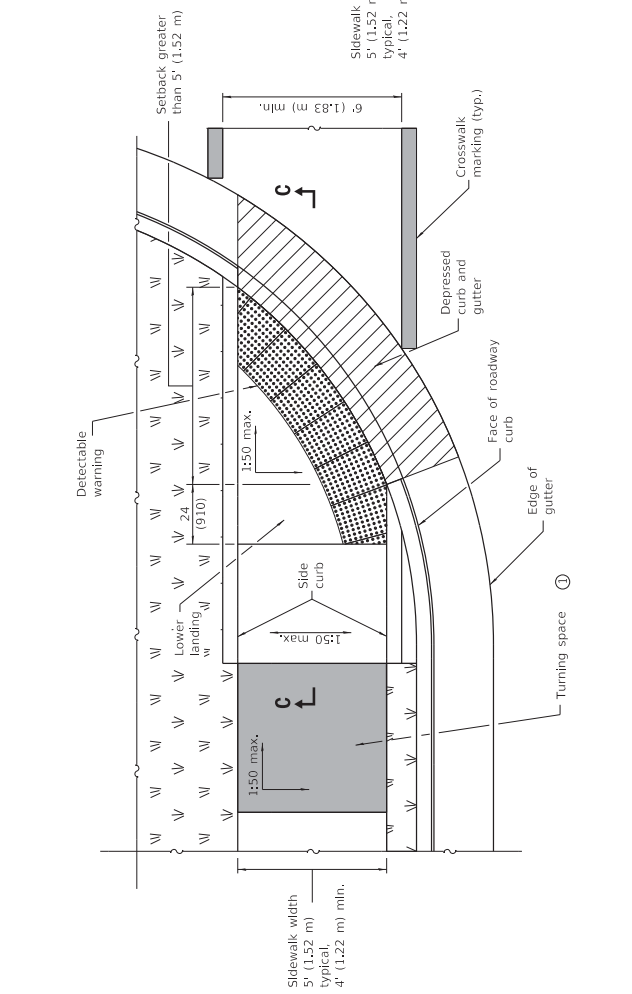
PASSED January 1, 2018
 Michael Board
 ENGINEER OF POLICY AND PROCEDURES

APPROVED January 1, 2018
 Matthew R. B...
 ENGINEER OF DESIGN AND ENVIRONMENT

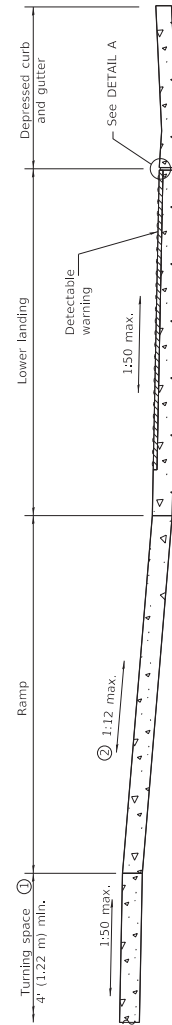
ISSUED 1-1-07



RAMP IN LANDSCAPED AREA
SETBACK > 5'



RAMP IN PAVED AREA
SETBACK > 5'



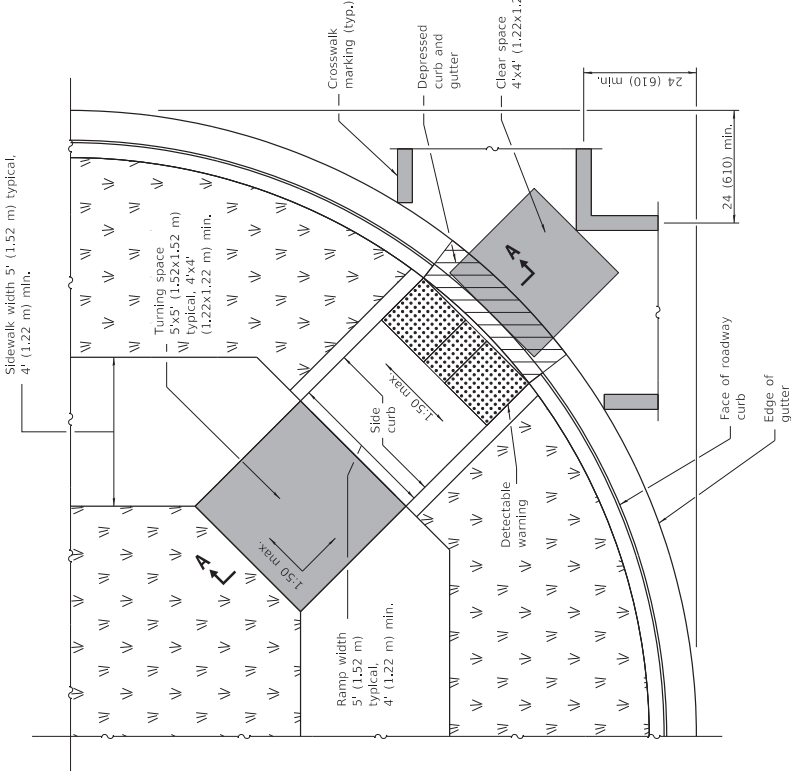
SECTION C-C

- ① Turning space not required for ramp slopes flatter than 1:20.
- ② The running slope of the curb ramp shall not require the ramp length to exceed 15' (4.5 m).

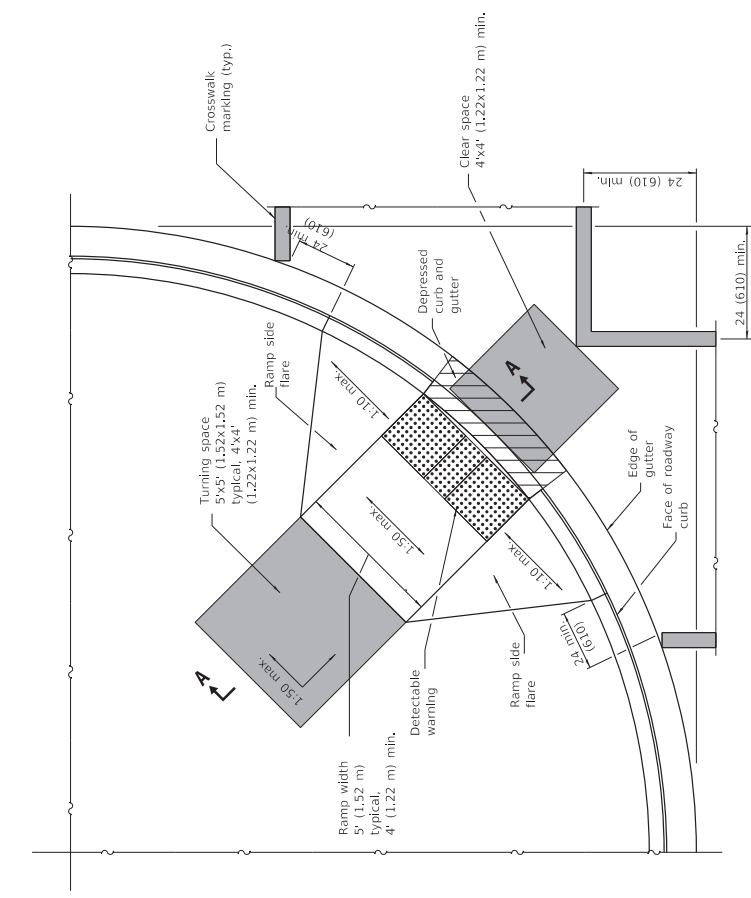
GENERAL NOTES
All slope ratios are expressed as units of vertical displacement to units of horizontal displacement (V:H).
Where the turning space is constrained on a side opposite a ramp, the minimum length of the turning space in the direction of the ramp-run shall be 5' (1.52 m).
Where 1:50 maximum slope is shown, 1:64 is preferred.
See Standard 606001 for details of depressed curb adjacent to curb ramp.
All dimensions are in inches (millimeters) unless otherwise shown.

PERPENDICULAR CURB RAMPS FOR SIDEWALKS
(Sheet 2 of 2)
STANDARD 424001-10

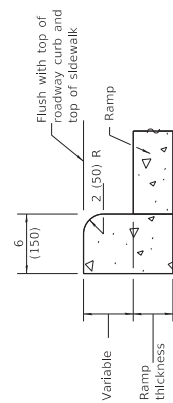
Illinois Department of Transportation PASSED APPROVED ENGINEER OF POLICY AND PROCEDURES ENGINEER OF DESIGN AND ENVIRONMENT	January 1, 2018 <i>Michael Beard</i> January 1, 2018 <i>Maureen P. Beck</i>	ISSUED 1-1-07
	2018 2018	



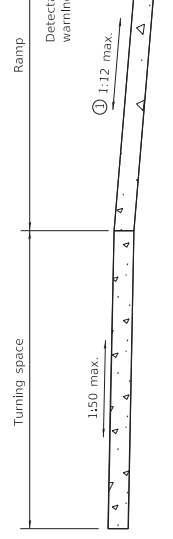
RAMP IN LANDSCAPED AREA



RAMP IN PAVED AREA



SIDE CURB DETAIL



SECTION A-A

① The running slope of the curb ramp shall not require the ramp length to exceed 15' (4.5 m).



DETAIL A

GENERAL NOTES

This Standard shall only be used for curb radii of 20 ft. (6.1 m) or greater.
 Where the turning space is constrained on a side opposite a ramp, the minimum length of the turning space in the direction of the ramp-run shall be 5' (1.52 m).
 Where 1:50 maximum slope is shown, 1:64 is preferred.

All slope ratios are expressed as units of vertical displacement to units of horizontal displacement (V:H).
 See Standard 606001 for details of depressed curb adjacent to curb ramp.

All dimensions are in inches (millimeters) unless otherwise shown.

DATE	REVISIONS
1-1-18	Omitted diagonal slope at turning spaces.
1-1-15	Changed 'upper landing' to 'Turning space'. Added note reg. const. turning space.

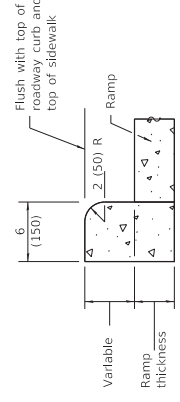
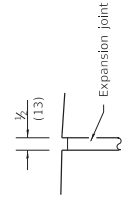
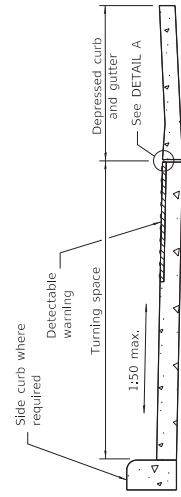
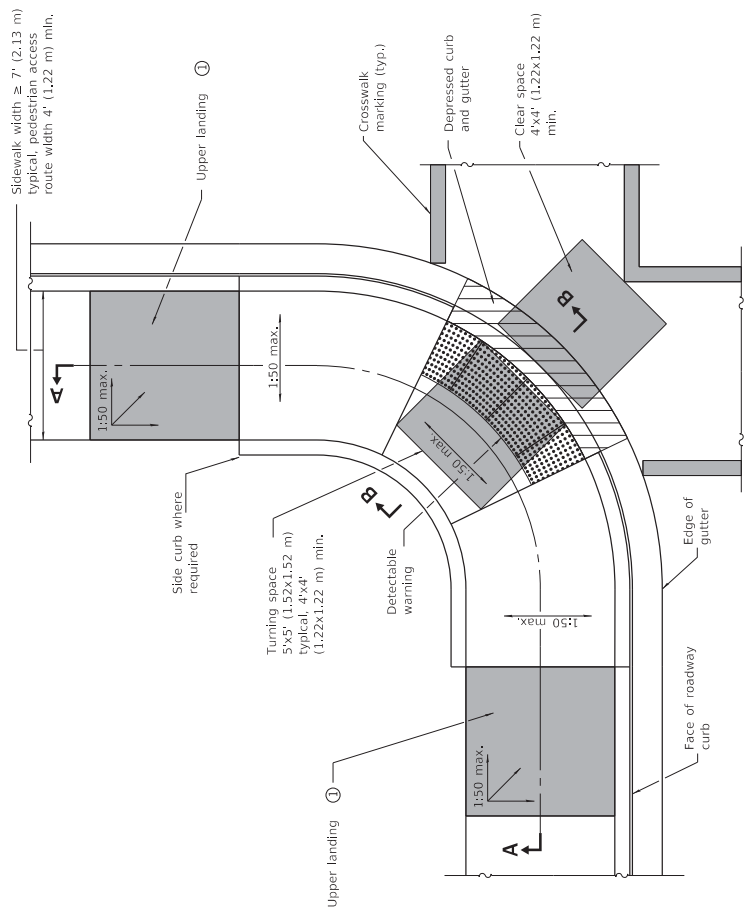
DIAGONAL CURB RAMPS FOR SIDEWALKS

STANDARD 424006-03

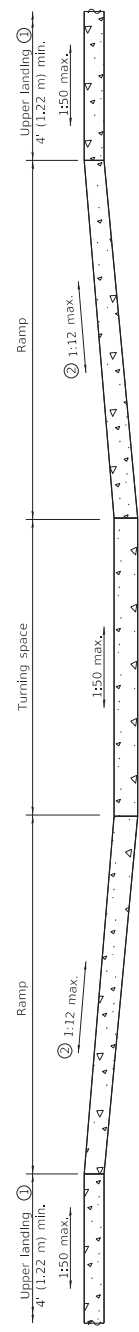
Illinois Department of Transportation
 PASSED January 1, 2018
 Michael Beard
 ENGINEER OF POLICY AND PROCEDURES
 APPROVED January 1, 2018
 Matthew R. Beck
 ENGINEER OF DESIGN AND ENVIRONMENT

ISSUED 1-1-12

Sidewalk width $\geq 7'$ (2.13 m) typical, pedestrian access route width 4' (1.22 m) min.



CORNER PARALLEL CURB RAMP



- SECTION A-A**
- ① Upper landing(s) not required for ramp slopes flatter than 1:20.
 - ② The running slope of the curb ramp shall not require the ramp length to exceed 15' (4.5 m).

GENERAL NOTES
 All slope ratios are expressed as units of vertical displacement to units of horizontal displacement (V:H).

Where the turning space is constrained on a side opposite a ramp, the minimum length of the turning space in the direction of the ramp-run shall be 5' (1.52 m).

Where 1:50 maximum slope is shown, 1:64 is preferred.

See Standard 606001 for details of depressed curb adjacent to curb ramp.

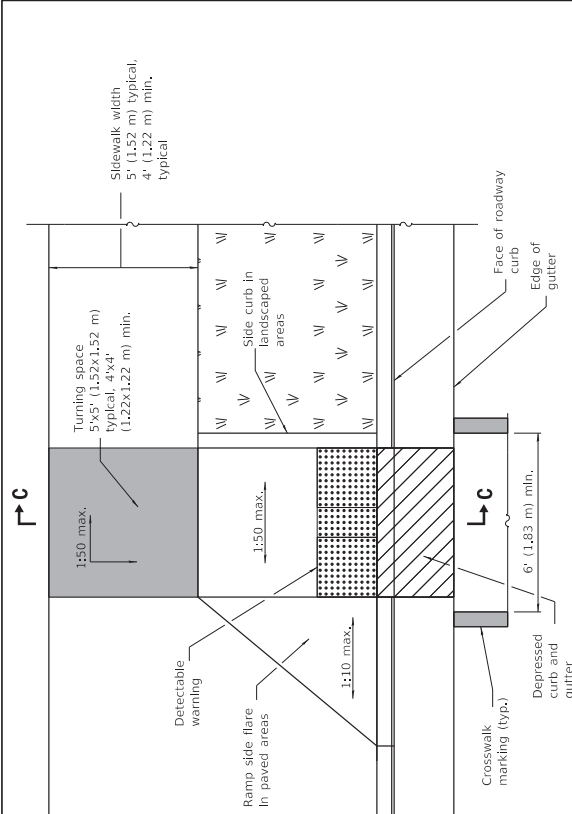
All dimensions are in inches (millimeters) unless otherwise shown.

DATE	REVISIONS
1-1-17	Revised sidewalk width to include 24 (610) buffer behind curb.
1-1-15	Changed 'Lower landing' to 'Turning space'. Added x-walk markings. Added note ②.

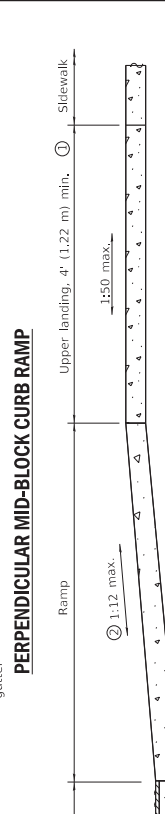
Illinois Department of Transportation PASSED <i>Michael Bond</i> ENGINEER OF POLICY AND PROCEDURES APPROVED <i>Maureen M. Bales</i> ENGINEER OF DESIGN AND ENVIRONMENT	ISSUED 1-1-12 JANUARY 1, 2017
	JANUARY 1, 2017

CORNER PARALLEL CURB RAMPS FOR SIDEWALKS

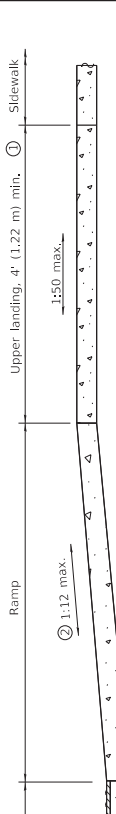
STANDARD 424011-03



PARALLEL MID-BLOCK CURB RAMP

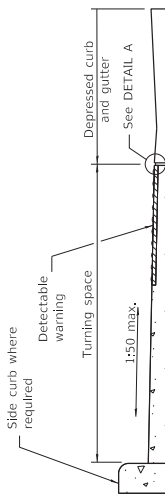


PERPENDICULAR MID-BLOCK CURB RAMP

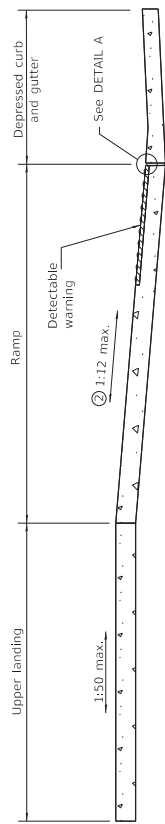


SECTION A-A

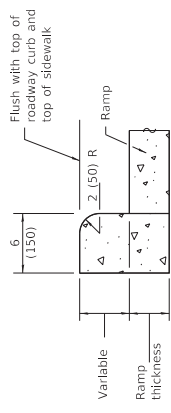
- ① Upper landing(s) not required for ramp slopes flatter than 1:20.
- ② The running slope of the curb ramp shall not require the ramp length to exceed 15' (4.5 m).



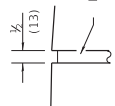
SECTION B-B



SECTION C-C



SIDE CURB DETAIL



DETAIL A

GENERAL NOTES

All slope ratios are expressed as units of vertical displacement to units of horizontal displacement (V:H).
Where the turning space is constrained on a side opposite a ramp, the minimum length of the turning space in the direction of the ramp-run shall be 5' (1.52 m).
Where 1:50 maximum slope is shown, 1:64 is preferred.

See Standard 606001 for details of depressed curb adjacent to curb ramp.

All dimensions are in inches (millimeters) unless otherwise shown.

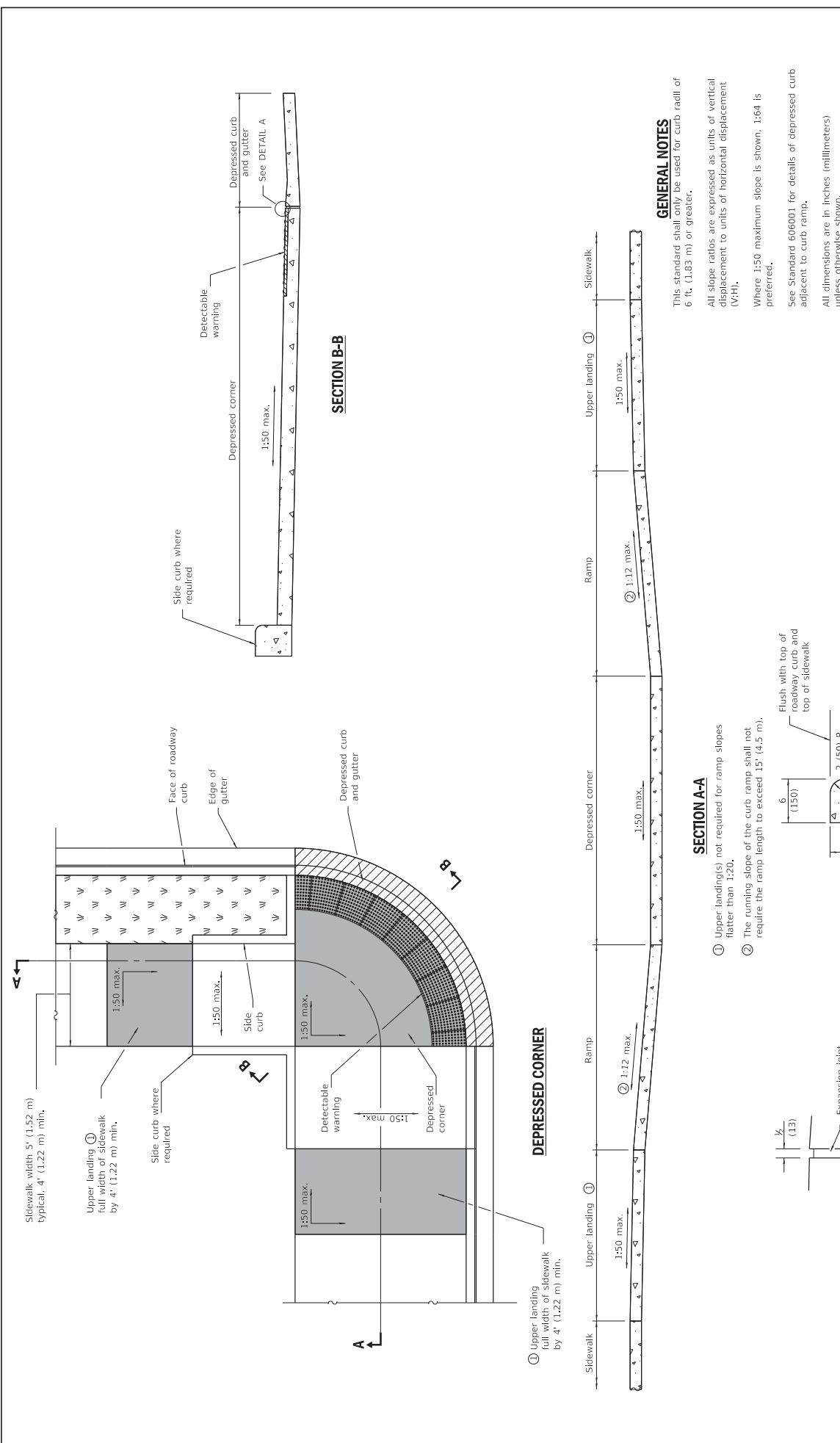
REVISIONS	
DATE	REVISIONS
1-1-18	Omitted diagonal slope at turning spaces and upper landings.
1-1-17	Revised sidewalk width to include 24 (610) buffer behind curb.

MID-BLOCK CURB RAMPS FOR SIDEWALKS

STANDARD 424016-04

Illinois Department of Transportation
 PASSED January 1, 2018
 Michael Board
 ENGINEER OF POLICY AND PROCEDURES
 APPROVED January 1, 2018
 Haucen in Blue
 ENGINEER OF DESIGN AND ENVIRONMENT

ISSUED 1-1-12



GENERAL NOTES

This standard shall only be used for curb radii of 6 ft (1.83 m) or greater.
 All slope ratios are expressed as units of vertical displacement to units of horizontal displacement (V:H).
 Where 1:50 maximum slope is shown, 1:64 is preferred.
 See Standard 606001 for details of depressed curb adjacent to curb ramp.
 All dimensions are in inches (millimeters) unless otherwise shown.

SECTION A-A

- ① Upper landing(s) not required for ramp slopes flatter than 1:20.
- ② The running slope of the curb ramp shall not require the ramp length to exceed 15' (4.5 m).

DEPRESSED CORNER FOR SIDEWALKS

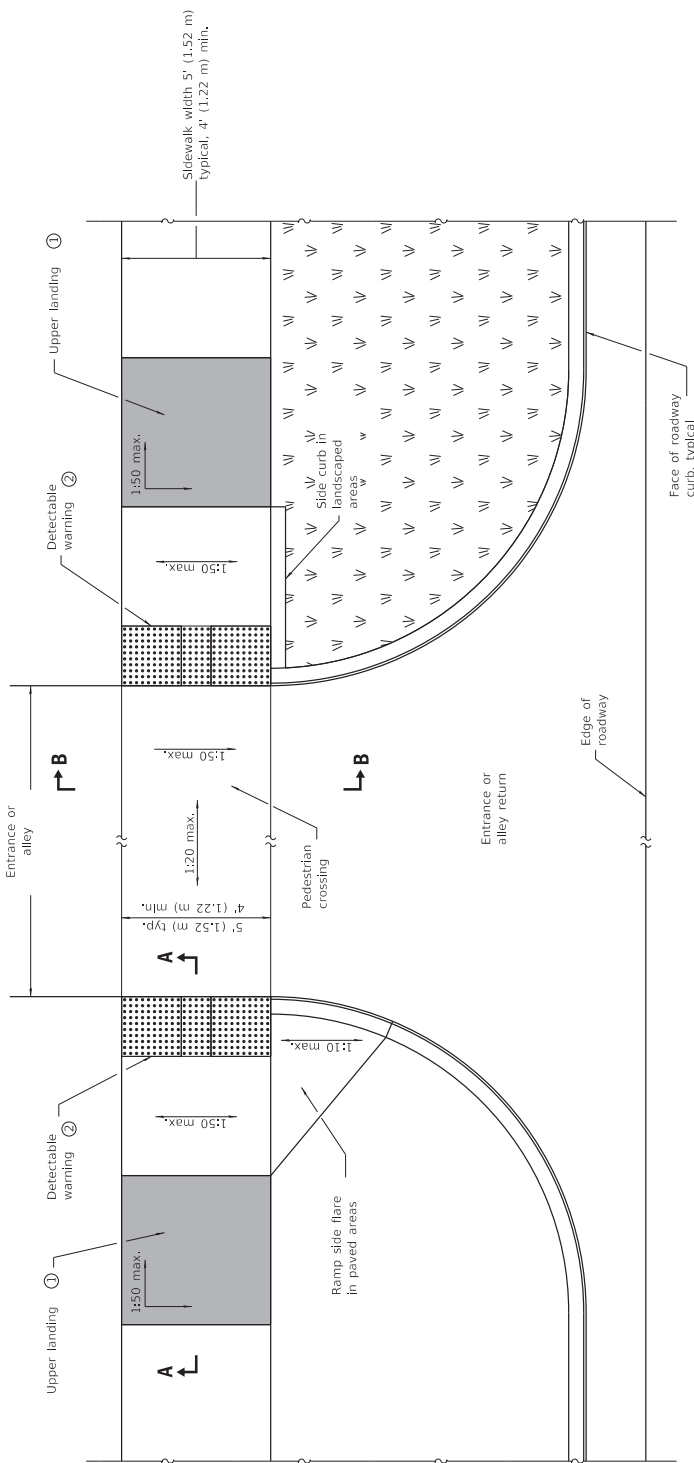
DATE	REVISIONS
1-1-18	Omitted diagonal slope at turning spaces and upper landings.
1-1-15	Added note ②.

STANDARD 424021-04

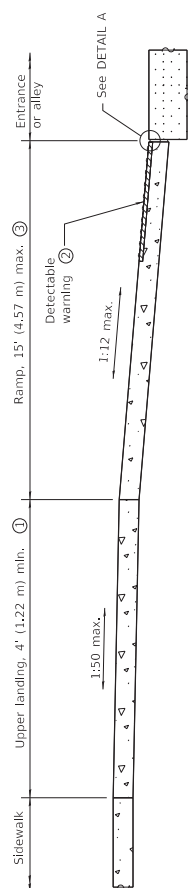
Illinois Department of Transportation

PASSED
 January 1, 2018
 Michael Board
 ENGINEER OF POLICY AND PROCEDURES
 APPROVED
 January 1, 2018
 [Signature]
 ENGINEER OF DESIGN AND ENVIRONMENT

ISSUED 1-1-12

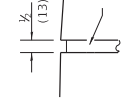


ENTRANCE / ALLEY PEDESTRIAN CROSSING

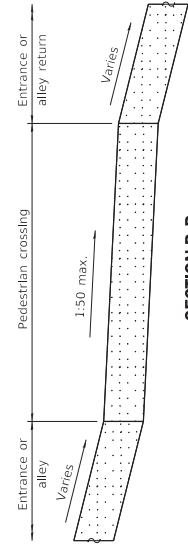


SECTION A-A

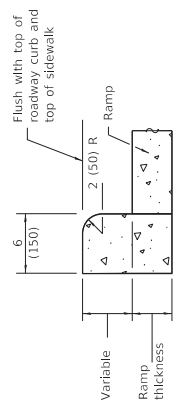
① Upper landing not required for ramp slopes flatter than 1:20.



DETAIL A



SECTION B-B



SIDE CURB DETAIL

GENERAL NOTES

All slope ratios are expressed as units of vertical displacement to units of horizontal displacement (V:H).

Where 1:50 maximum slope is shown, 1:64 is preferred.

All dimensions are in inches (millimeters) unless otherwise shown.

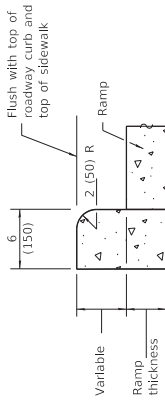
APPROVED January 1, 2018 <i>Michael Board</i> ENGINEER OF POLICY AND PROCEDURES	ISSUED 1-1-12
	APPROVED January 1, 2018 <i>Maureen P. O'Neil</i> ENGINEER OF DESIGN AND ENVIRONMENT

DATE	REVISIONS
1-1-18	Omitted diagonal slope at upper landings.
1-1-13	Revised General Notes.

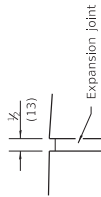
ENTRANCE / ALLEY PEDESTRIAN CROSSINGS

STANDARD 424026-02

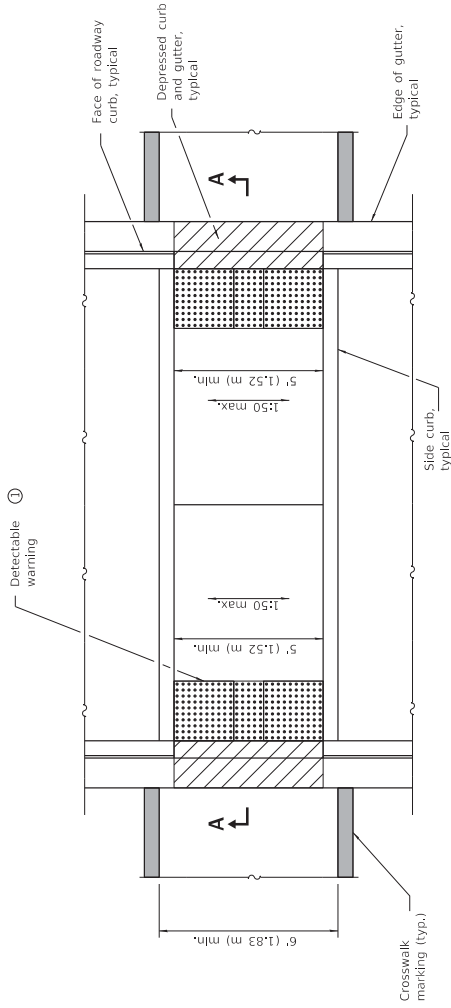
- ② Detectable warning shall only be installed at entrances/alleys with permanent traffic control devices (i.e. stop signs, signals).
- ③ Where possible, maintain the grade of the sidewalk across the entrance/alley to avoid the need for ramps and upper landings.



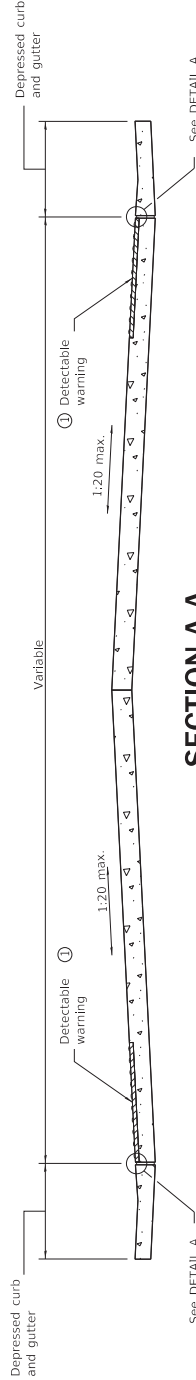
SIDE CURB DETAIL



DETAIL A



MEDIAN PEDESTRIAN CROSSING



SECTION A-A

① Omit detectable warnings when distance between back of curbs is less than 6' (1.83 m).

GENERAL NOTES

All slope ratios are expressed as units of vertical displacement to units of horizontal displacement (V:H).

Where 1:50 maximum slope is shown, 1:64 is preferred.

See Standard 606001 for details of depressed curb adjacent to curb ramp.

All dimensions are in Inches (millimeters) unless otherwise shown.

MEDIAN PEDESTRIAN CROSSINGS	
DATE	REVISIONS
1-1-12	Widened crosswalk to 6' (1.83 m) min. inside dimension. Revised General Notes.
1-1-12	New standard.

STANDARD 424031-01

Illinois Department of Transportation
 PASSED January 1, 2013
 Michael Bond
 ENGINEER OF POLICY AND PROCEDURES
 APPROVED January 1, 2013
 ENGINEER OF DESIGN AND ENVIRONMENT

ATTACHMENT C

1. RESOLUTION ADOPTING AN ADA TRANSITION PLAN AND ESTABLISHING AN ADA COORDINATOR
2. ADA GRIEVANCE PROCEDURE
3. ADA COMPLAINT/GRIEVANCE FORM

**THE CITY OF WATERLOO, ILLINOIS
GRIEVANCE PROCEDURE UNDER THE
AMERICANS WITH DISABILITIES ACT**

This Grievance Procedure is established to meet the requirements of the Americans with Disabilities Act of 1990 (ADA). It may be used by anyone who wishes to file a complaint alleging discrimination on the basis of disability in the provision of services, activities, programs, or benefits by the **City of Waterloo, Illinois**. The City's **Personnel Policy** governs employment-related complaints of disability discrimination.

The complaint should be in writing and contain information about the alleged discrimination such as name, address, phone number of complainant and location, date, and description of the problem. Alternative means of filing complaints, such as personal interviews or a tape recording of the complaint, will be made available for persons with disabilities upon request.

The complaint should be submitted by the grievant and/or his/her designee as soon as possible but no later than **60 calendar days** after the violation to:

NAME
TITLE (ADA Coordinator)
ADDRESS

Within **15 calendar days** after receipt of the complaint, **Name** or **his/her** designee will meet with the complainant to discuss the complaint and the possible resolutions. Within **15 calendar days** of the meeting, **Name** or **his/her** designee will respond in writing, and where appropriate, in a format accessible to the complainant, such as large print, Braille, or audio tape. The response will explain the position of the **City of Waterloo, Illinois**, and offer options for substantive resolution of the complaint.

If the response by **Name** or **his/her** designee does not satisfactorily resolve the issue, the complainant and/or his/her designee may appeal the decision within **15 calendar days** after receipt of the response to the **Street Committee** or its designee.

Within **30 calendar days** after receipt of the appeal, the **Street Committee** or its designee will meet with the complainant to discuss the complaint and possible resolutions. Within **30 calendar days** after the meeting, the **Street Committee** or its designee will respond in writing, and, where appropriate, in a format accessible to the complainant, with a final resolution of the complaint.

All written complaints received by **Name** or **his/her** designee, appeals to the **Street Committee** or its designee, and responses from these two offices will be retained by the **City of Waterloo, Illinois**, for at least **three years**.

City of Waterloo, IL - ADA Complaint / Grievance Form

Complainant: _____

Person Preparing Complaint (if different from Complainant): _____

Relationship to Complainant (if different from Complainant): _____

Street Address: _____

City: _____ State: _____ Zip: _____

Phone: (____) _____ E-mail: _____

Please provide a complete description of the specific complaint or grievance:

Please specify any location(s) related to the complaint or grievance (if applicable):

Please state what you think should be done to resolve the complaint or grievance:

Please attach additional pages as needed.

Please do not contact me personally.

Signature: _____ Date: _____

Return to: City of Waterloo, ADA Coordinator, 100 West 4th Street, Waterloo, IL 62298

Upon request, reasonable accommodation will be provided in completing this form, or copies of the form will be provided in alternative formats. Contact the ADA Coordinator at the address listed above or via Waterloo City Hall telephone (618)-939-8600.